

PLANNING COMMISSION
MEETING

In compliance with Americans with Disabilities Act, If you need special assistance to participate in this meeting, please contact the Community Development Director at (805) 524-1500 ext. 116, 48 hours prior to the meeting in order for the City to make reasonable arrangements to ensure accessibility to this meeting (28 CFR 35.102-35.104 ADA Title II).

No New Business will be Considered by the City Council/Planning Commission after the Hour of 11:00 p.m. unless a Majority of the City Council/Planning Commission Determines to Continue beyond that Hour.

Memorandums: Memorandums relating to agenda items are on file in the Planning Department. If you have questions regarding the agenda, you may call the Planning Dept. (805) 524-1500 ext. 116 or visit the Planning Dept. in City Hall for information. Materials related to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the Planning Dept. in City Hall during normal business hours.

AGENDA

- | ITEM | REFERENCE |
|---|-----------|
| 1. CALL TO ORDER | |
| 2. PLEDGE OF ALLEGIANCE | |
| 3. ORAL COMMUNICATIONS | |
| This is the opportunity for citizen presentations or comments not related to agenda items, but within responsibility of the City Council/Planning Commission (Each speaker is limited to 5 minutes). | |
| 4. PLANNING COMMISSION CONSENT CALENDAR | |
| 4a. Minutes of the October 23, 2013 Regular joint meeting with Planning Commission/Parks Commission and Film Commission. | Copy |
| 5. PUBLIC HEARINGS - NONE | |
| 6. BUSINESS ITEM | |
| 6A. Ventura Street Design Guideline Review | Memo |
| <p>Purpose: Determine if the Ventura Street Design Guidelines should be revised and if so, provide a recommendation to Council for proposed revisions.</p> <p>Recommendation: Identify what components of the Ventura Street Design Guideline should be revised if it is determined revisions are needed.</p> | |

7. REPORTS and COMMUNICATIONS

7a. Community Development Director

Oral

7b. Planning Commission

Oral

8. ADJOURNMENT

8a. The Planning Commission adjourns to the next regular Planning Commission meeting scheduled for December 18, 2013, 6:30 p.m., in the City Council Chambers, 250 Central Ave., Fillmore, CA 93015.

**Next Regular City Meeting
December 10, 2013**

PLEASE NOTE: If you challenge the actions of the Planning Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in the public notice, or in written correspondence delivered to the Planning Commission at, or prior to, the public hearing (Calif. Gov't Code § 65009).

Any legal action by an applicant seeking to obtain judicial review of the Planning Commission's decision on a hearing listed on this agenda may be subject to the 90-day filing period of, and governed by, Code of Civil Procedure Section 1094.6.

**JOINT PLANNING COMMISSION/PARKS COMMISSION
AND
FILLMORE COMMISSION
MINUTES**

1. CALL TO ORDER

Chair Tim Holmgren called the Planning Commission meeting to order at 6:33 p.m. and led the assembly in the Pledge of Allegiance. Planning Commissioners present were: Tim Holmgren, Christopher Hoy, Mark Greenwell, Robert Smith, and Jayme Laber. Parks Commission members present were: Camilla Conaway, Geri Lee, Ken Palmer, Film Commission member was Carrie Broggie. City Council Liaison to Film Commission present was Steve Conaway. City Staff present was: David Rowlands, Planning Director, Kevin McSweeney, Deputy City Attorney Kathrine Phelan.

NO QUORUM for Film Commission

2. PLEDGE OF ALLEGIANCE

3. ORAL COMMUNICATIONS

There were no public comments made.

4. PLANNING COMMISSION CONSENT CALENDAR

4a. Minutes of the September 25, 2013 Regular Planning Commission Meeting.

Planning Commissioner Hoy motioned to accept the minutes of September 25, 2013 meeting as submitted, Member Smith seconded with an amendment to the minutes to add the Commissioner Smiths comments on Item 6a, Screening 13-01, Family Dollar, all Planning Commissioners were in favor.

5. PUBLIC HEARINGS - NONE

6. JOINT CITY COUNCIL/PLANNING COMMISSION AND FILM COMMISSION TRAINING SESSION

6a. AB 1234 ethics training

Planner Director, Kevin McSweeney, introduced Deputy City Attorney, Kathrine Phelan. The Deputy City Attorney presented a power point presentation.

7:26 p.m. Break

7:36 p.m. Kathrine Phelan continued the power point presentation.

7. REPORTS and COMMUNICATIONS

- 7a. Community Development Director -None
- 7b. Planning Commission -None
- 7c. Parks Commission-None
- 7d. Film Commission - None

8. ADJOURNMENT

- 8a. The Planning Commission adjourned at 8:36 pm on October 23, 2013 until the next regular City Council meeting scheduled for November 20 , 2013, 6:30 p.m., in the City Council Chambers, 250 Central Ave., Fillmore, CA 93015.
- 8b. The Parks Commission adjourned at 8:36 pm on October 23, 2013 until the next regular Parks Commission meeting scheduled for November 18, 2013, 6:30 p.m., in the City Council Chambers, 250 Central Avenue, Fillmore, CA 93013

Kevin McSweeney
Planner Director



CITY OF FILLMORE
CENTRAL PARK PLAZA
250 Central Avenue
Fillmore, California 93015-1907
(805) 524-3701 • FAX (805) 524-5707

TO: Planning Commission

FROM: Kevin McSweeney,
Planning and Community Development Director 

DATE: November 20, 2013

Subject: Business Item, Ventura Street Design Guideline Review

REQUEST

Determine if the Ventura Street Design Guidelines should be revised and if so, provide a recommendation to the City Council for proposed revisions.

DISCUSSION

City Council member, Doug Tucker, serves as a City Council liaison to the Planning Commission and noted the Planning Commissioners discussions and desires to revisit the Ventura Street Design Guidelines.

Council member Tucker requested the City Council direct staff to begin working with the Planning Commission on this issue and City Council concurred.

ANALYSIS

The Ventura Street Design Guidelines were adopted on March 14, 1989. Since that time, projects along the highway have been reviewed in accordance to the guidelines.

The design guidelines state that buildings along Highway 126 should select from two architectural styles: Craftsman or Railroad. Buildings should also have certain components such as colonnades/arcade, terminus detailed columns and windows, varied roof lines and detailed signs.

The intent of the guidelines is to set a vision that buildings should reflect Fillmore's unique character that separates and distinguishes Fillmore from other cities in California i.e. Fillmore identity.

The guidelines are not regulations and therefore not mandatory. For that reason, there is no building constructed along Hwy 126 that strictly adheres to every detailed requirement. Ventura Street Design Guidelines page 7 states, "The examples and sketches provided are not intended to be copied or duplicated."

General Plan Land Use Element 2005

If the Planning Commissioners desire to revise the guideline, the revision will need to conform to the attached General Plan Land Use Element 2005, goals, policies and implementation under the category called Urban Form.

Parameters:

Before details of revisions are discussed, there should be a general discussion about what is the Planning Commission objective in their review. What is the vision and what type components should be in the revision such as highway 126 access, landscaping, outdoor people spaces, parking, architecture, signage, entrance Gateway signs and other.

FISCAL IMPACT

It is undetermined what the cost of a revised guideline will be at this time. Prior to any assigned work, City staff will provide a cost estimate.

CITY COUNCIL GOAL

The review of the guideline is identified as a City Council goal, Maintain and Improve City Infrastructure, #3 Beautification.

RECOMMENDATION

The Planning Department Staff recommends the Commission take the following action:

1. Direct staff regarding any objectives on revision to the guidelines and report to the City Council.

Alternative recommendations

2. Determine that the Ventura Street Design Guideline is sufficient and report to the City Council

(or)

3. Continue to study the subject matter

ATTACHMENTS

1. City Council minutes, March 14, 1989
2. Ventura Street Design Guideline.
3. General Plan Land Use Element 2005

Attachment #1

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FILLMORE CITY COUNCIL - Page 3

MARCH 14, 1989

ACTION

AUTH INSTALL
STOP SIGN
4TH & FINE

Motion: Campbell, Second: McMahan, UNANIMOUS, to direct the Chief of Police to authorize the installation of a "STOP" sign at the intersection of Fourth Street and Fine Street. The staff report was presented by City Engineer Chuck Mink.

WELL #4 POND
RENOVATION
BIDS

Staff report was presented by Bert Rapp of the City Engineering Department. Mr. Rapp advised Council the purpose of the renovation is to restore percolation to the pond. Mr. Rapp also noted new fencing will be set back 20 feet to allow for landscaping to the area. Council discussion ensued. The bids, which were opened March 7, 1989 went to the low bidder Hodgson Construction. Mr. Rapp reported this is an experienced and reputable firm, qualified to perform this work.

ACTION

AWARD WELL #4
POND RENOV
BID

Motion: Lee, Second: Campbell, UNANIMOUS, to award the contract for Well #4 Pond Renovation, Spec. No. 88-5 to Hodgson Contractors, Inc. in the amount of \$42,945.00.

ACTION

APPR VENTURA
ST DESIGN
GUIDELINES

Motion: Murphy, Second: Lee, UNANIMOUS, to approve the Ventura Street Design Guidelines and authorize expenditure of Redevelopment funds to the low bidder, Cole Litho, to print the guidelines document with two full-color plates. The staff report was presented by City Planner Krause and the Planning Commission was commended for the work involved in obtaining the guidelines.

ACTION

APPR FILM
PERMIT FOR
HKM PROD

Motion: McMahan, Second: Campbell, UNANIMOUS, to approve the request of HKM Productions to film in the City on March 20, 1989, with approved police and fire personnel on duty. The staff report was presented by the City Clerk. Speaking to the matter was Lt. Diaz, Fire Chief Askren and Kelly Sullivan of HKM Productions.

CHAMBER TO
HOLD FESTIVAL
ON RR PROP

Mary Taylor, representing the Fillmore Chamber of Commerce, brought Councilmembers up to date on the new site for Fillmore's May Festival. The Chamber has leased railroad property, east of Dewey's Garden Center, for the festival to be held the weekend of May 18 - 21, 1989. Mrs. Taylor noted the conditions requested by the railroad and asked the City to assist the Chamber with scraping and preparing the area. Council discussion ensued and Mrs. Taylor asked Councilmembers if any notification of nearby residents would be required. Councilmembers agreed it would be a nice thing to do even though it is not required. Other discussion included dates for the conclusion of the downtown sidewalk repair and City Engineer Mink noted the project could be finished in time for the parade. It was also suggested that more police may be required for the new location.

LTR RE REMOVE
TWO HR PKG
ON CENTRAL

Lt. Diaz presented the request of several Central Avenue merchants in the area south of Santa Clara to remove the two hour parking from the front of their business establishments. Lt. Diaz noted his concern with the City changing policy frequently on this type of request setting precedence in other areas. Council discussion ensued and Lt. Diaz was requested to survey merchants for their preference.



CITY OF

FILLMORE

VENTURA STREET DESIGN GUIDELINES

C R E D I T S

FILLMORE CITY COUNCIL

Delores Day, Mayor
John Murphy
Roger Campbell
Scott Lee
Mike Mc Mahan

PAST COUNCIL PARTICIPANTS

Gary Creagle
Pat Quinn

FILLMORE PLANNING COMMISSION

John Porter, Chairman
C. Ked Creed
Craig Ewing
Vance K. Johnson
Eric Sakowicz
Enrique Villasenor
Frank Wahl

FILLMORE CITY STAFF

Mitch Stone, City Planner
Mary Ann Krause
Director of Community Development
Linda Nash
Planning Secretary, Deputy City Clerk

GUIDELINES PREPARED BY

Mainstreet Architects and Planners
Ventura, California
Nicholas Deitch, Principal in Charge

GRAPHIC DESIGN AND LAYOUT BY

Duane Eells Design
Ventura, California

Attachment #2



T A B L E O F C O N T E N T S

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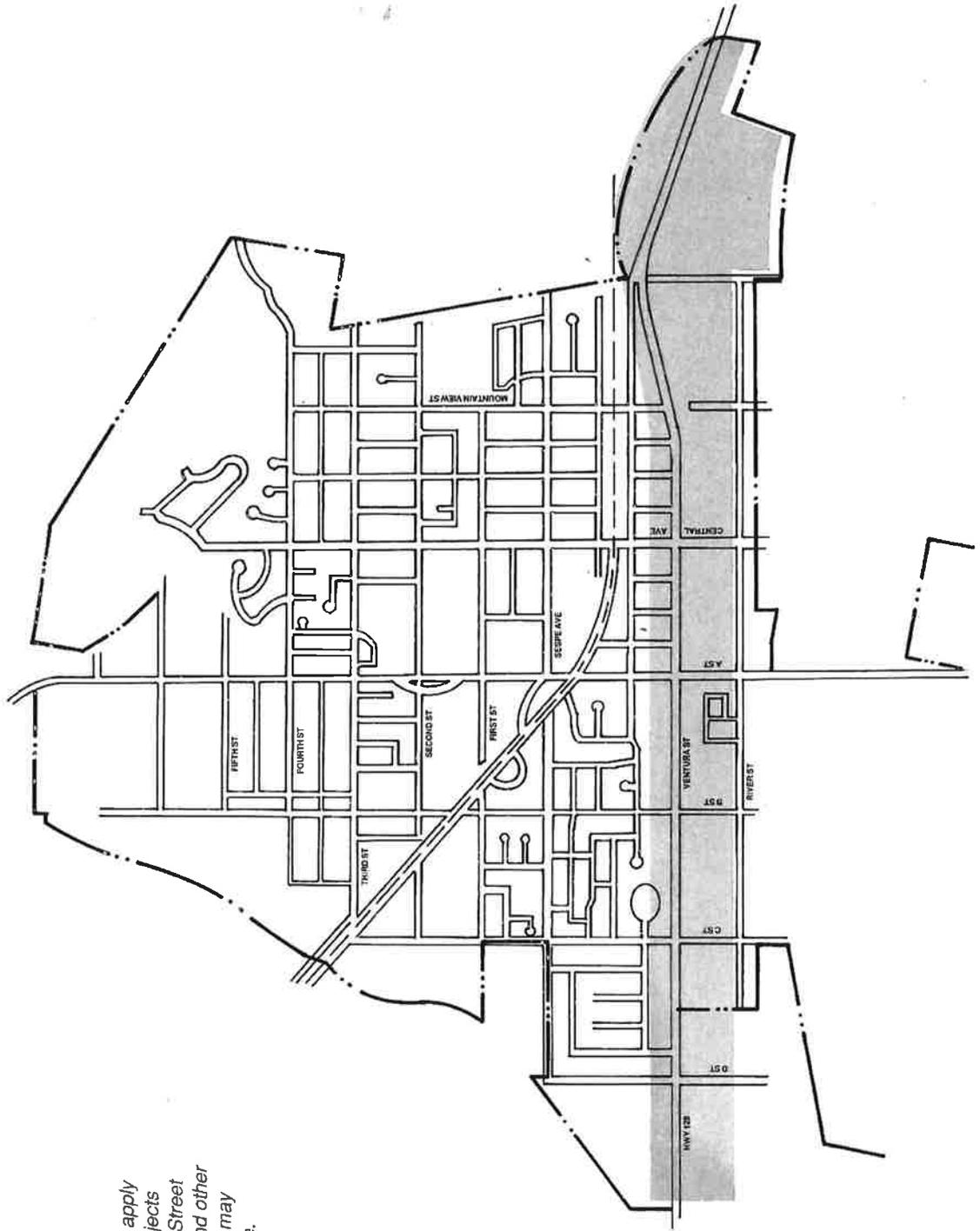
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Tenant Secondary Signs 28



GUIDELINES AREA MAP

These guidelines apply specifically to projects abutting Ventura Street (shaded area), and other areas as the City may deem appropriate.





I N T R O D U C T I O N

On November 8, 1988 the City Council of the City of Fillmore adopted two architectural themes to be incorporated into Design Guidelines for new development and existing development undergoing extensive remodeling or additions along Highway 126 (Ventura Street). The themes were selected from several alternatives studied by the Planning Commission, City Staff, and the consulting firm of Mainstreet Architects and Planners. These two themes were judged to be most representative of Fillmore's architectural roots and of the kind of image that the City would like to create for itself in the future. The themes, referred to as "Railroad" and "Craftsman", are based upon an eclectic assortment of architectural references from Fillmore's past, some of which have been destroyed, but many of which exist today throughout the City. These guidelines are intended to define the elements which will contribute to the creation of these themes and incorporate them into new site and building design standards for projects occurring along Highway 126, as well as any other areas that the City may choose to apply them.

It is intended that developers and architects will use these guidelines as a tool to assist them in creating projects of excellence and in harmony with Fillmore's unique historic character and scale.



S U B M I T T A L R E Q U I R E M E N T S

All new construction in a C-3 (General Commercial) zone requires the approval of a Planned Development Permit from the Planning Commission. Some projects will also require a Conditional Use Permit. Staff review for compliance with the standards set forth in the zoning code and conformance with these guidelines occurs prior to scheduling for a hearing before the Commission.

The City of Fillmore has specific standards to guide the submittal of applications for planning permit entitlements. These requirements describe such matters as the content, size, and form of site plans, landscape plans, and architectural elevations. To obtain a complete application package with the necessary forms and applications, please contact the Fillmore Planning Department at (805) 524-3701.

All project applications for a Planned Development Permit must include the following exhibit materials:

1. Project Site Plan:
Indicating all setbacks, easements, proposed planter areas, paved areas, building footprints, driveways, site lighting, etc.
2. Building Floor Plan(s):
Indicating general building configuration, doors, windows, covered walks, etc.

3. Building Elevations:
Showing all exterior design features, materials, sign areas and approximate locations, overall dimensions, etc.
4. Landscape Plan
Showing location of all landscape materials (identified by common name), indicating all land berming and mounding, hardscape features, etc.

5. Color and Materials Board
With photographs and/or actual material samples and color chips of all proposed exterior materials.

6. Illustrated Sign Program
Clearly identifying all sign locations (both on the building and on the site), maximum sizes, proposed materials, and method of illumination.

IMPLEMENTATION AND PROCEDURES

Projects occurring within the Design Guidelines Area shall be reviewed by the Planning Department and the Planning Commission for consistency with these guidelines as well as all requirements of current zoning and other city ordinances. Applicants are encouraged to submit preliminary design studies of proposed projects for conceptual review prior to submitting a formal application for a Planned Development and/or Conditional Use Permit.



ARCHITECTURAL THEMES

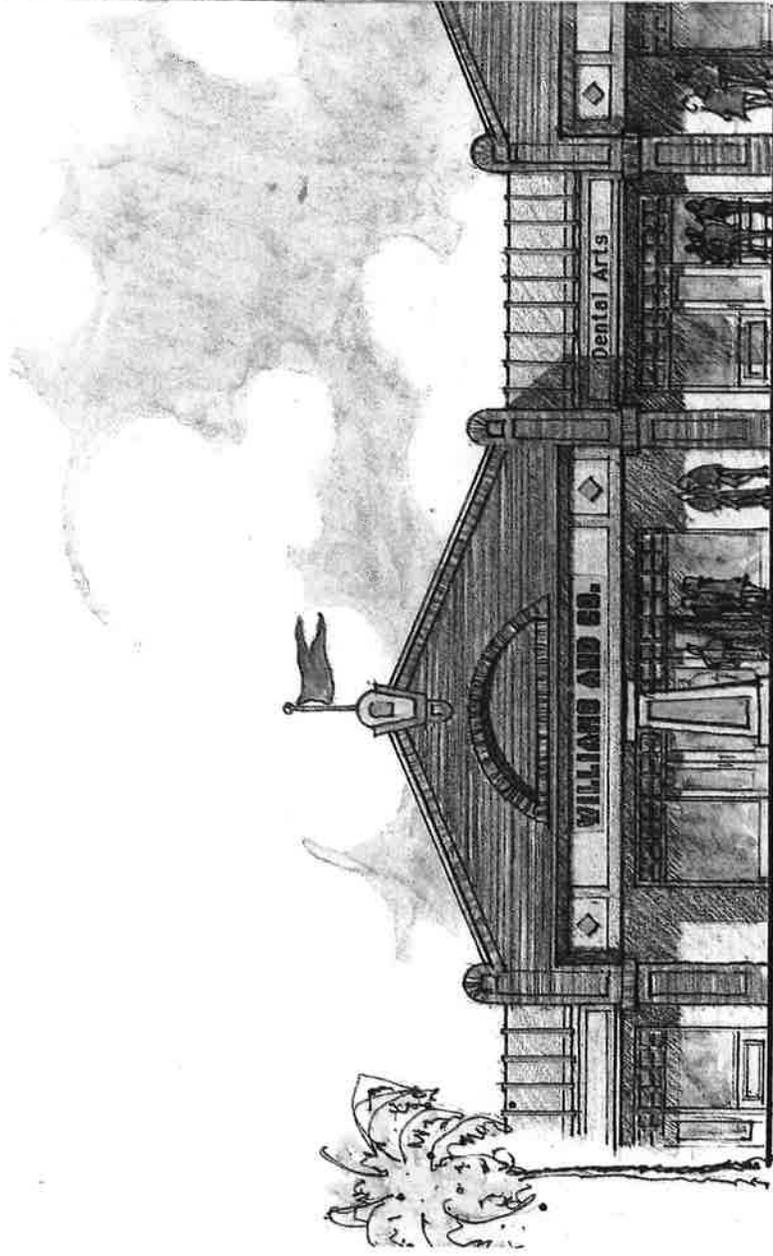
These architectural themes are intended to establish a “stylistic realm” within which various projects are to be designed. The purpose is to assure a relatedness between individual projects along the highway and a sense of “belonging and place” as they relate to the rest of the City of Fillmore. Both themes offer a wide range of design opportunities for the variety of specific user needs while assuring a lasting compatibility from project to project. It is recommended that the project designer spend some time studying the contextual references in the City of Fillmore by which these Guidelines were inspired.

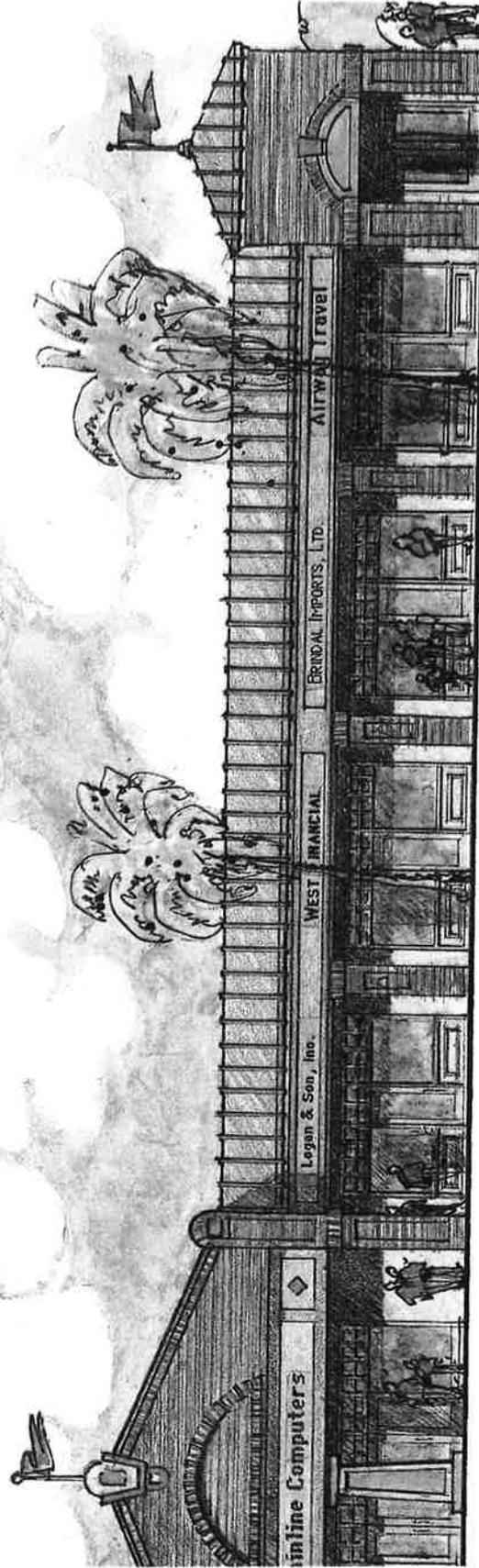
Following is a discussion of each theme along with sketch examples of the design elements and features associated with each. The examples and sketches provided are not intended to be copied or duplicated in project proposals, but are provided to help clarify the criteria and intention of these guidelines.



T H E R A I L R O A D T H E M E

The railroad played an important role in Fillmore's history, being one of the primary catalysts for its existence, and as such is used here as a source for creative historical reference. However, this theme does not seek to emulate the style and materials of the railroad buildings particular to Fillmore's history, which were typically simple wood clapboard buildings (the City's original Southern Pacific Railroad depot, built in 1888, can be seen in Downtown Fillmore on the north side of Main Street). Rather, this theme is based upon references found in many of the older commercial buildings (primarily brick) in Fillmore's downtown area, typically built between 1900 and 1930, mixed with traditional streetscape elements (such as iron benches, light standards, and bollards) to create the impressions and character of "old-time" railway stations and other early 20th century commercial vernacular. The architectural impression should be one of permanence and enduring quality.



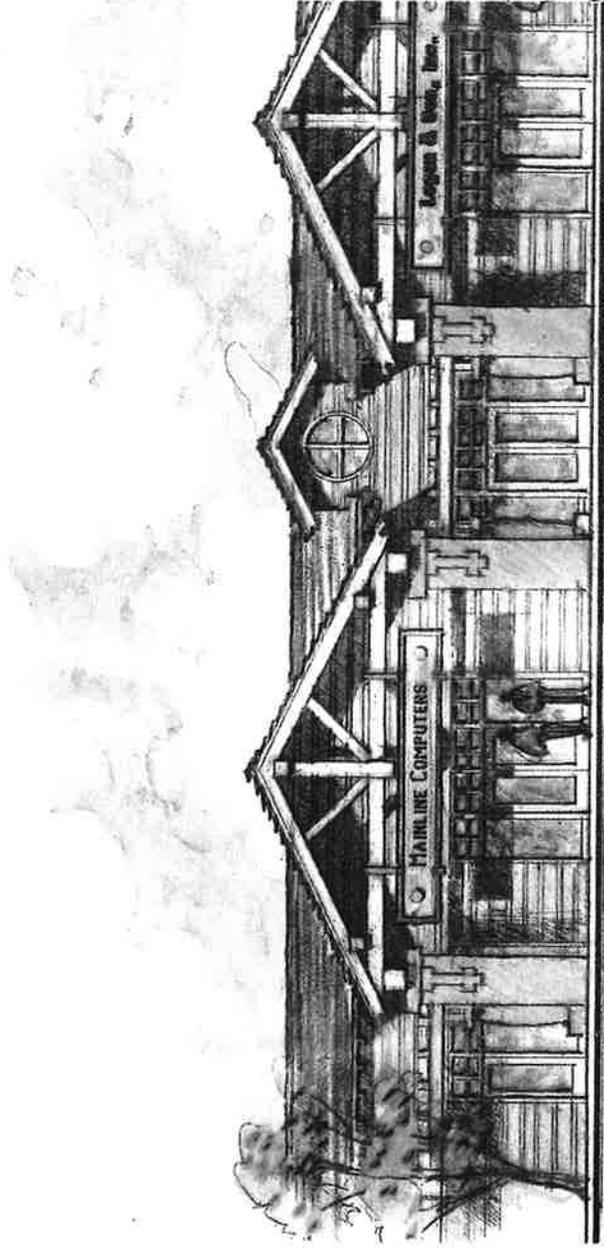


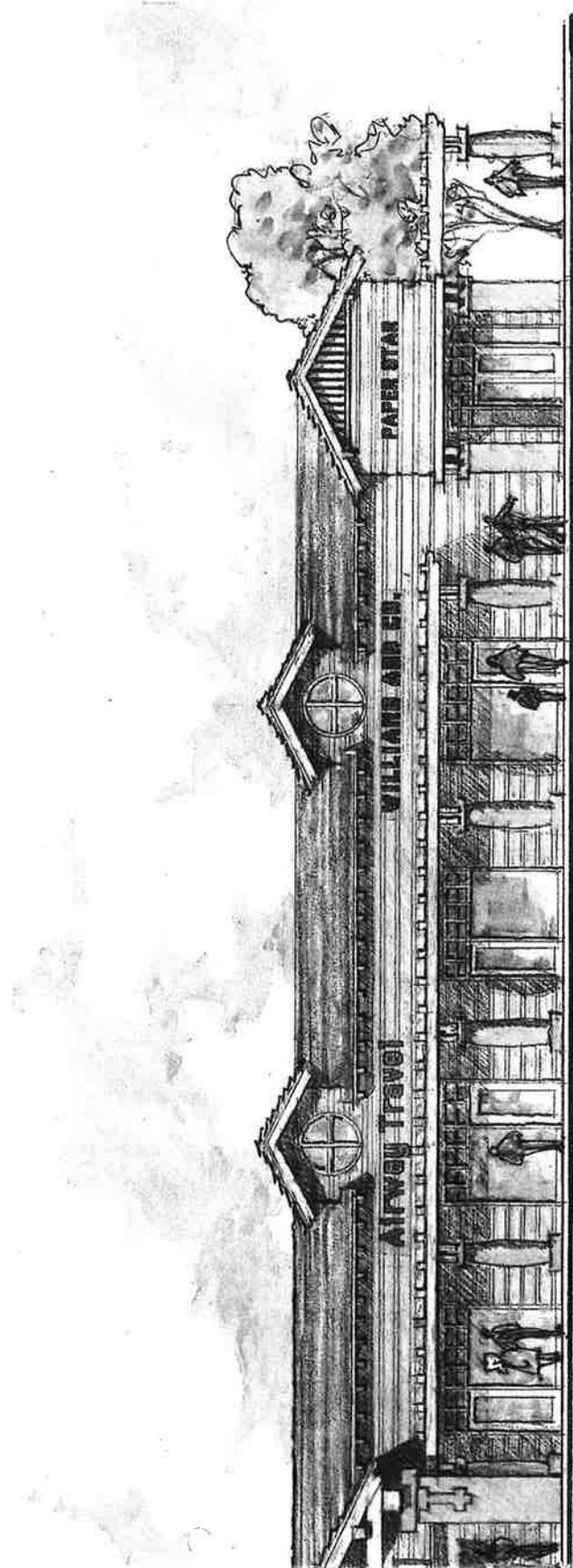


T H E C R A F T S M A N T H E M E

While the Railroad Theme emphasizes Fillmore's early 20th Century commercial architecture, the Craftsman Theme represents the domestic architecture that predominated in Fillmore during roughly the same time period. Many fine examples of these modest homes, often called Craftsman or California Bungalows, can be found on Fillmore, Saratoga, and Clay streets, north-east of the old downtown. The Craftsman aesthetic emphasized low, ground-hugging designs, naturalistic color schemes and building materials. Typically, these buildings showed off, rather than hid structural elements, such as rafter ends, trusses, and attic vents. Design elements were often somewhat massive and exaggerated in scale to celebrate their obvious functions.

Interpreting this style for commercial architecture will require attention to details, features, and massing not often considered in retail or office building design. Typically, the scale of projects will far exceed the scale of the traditional craftsman house. These guidelines do not seek to require reconstructions of authentic residential scale, but rather to create new and exciting projects true to the spirit of the style.





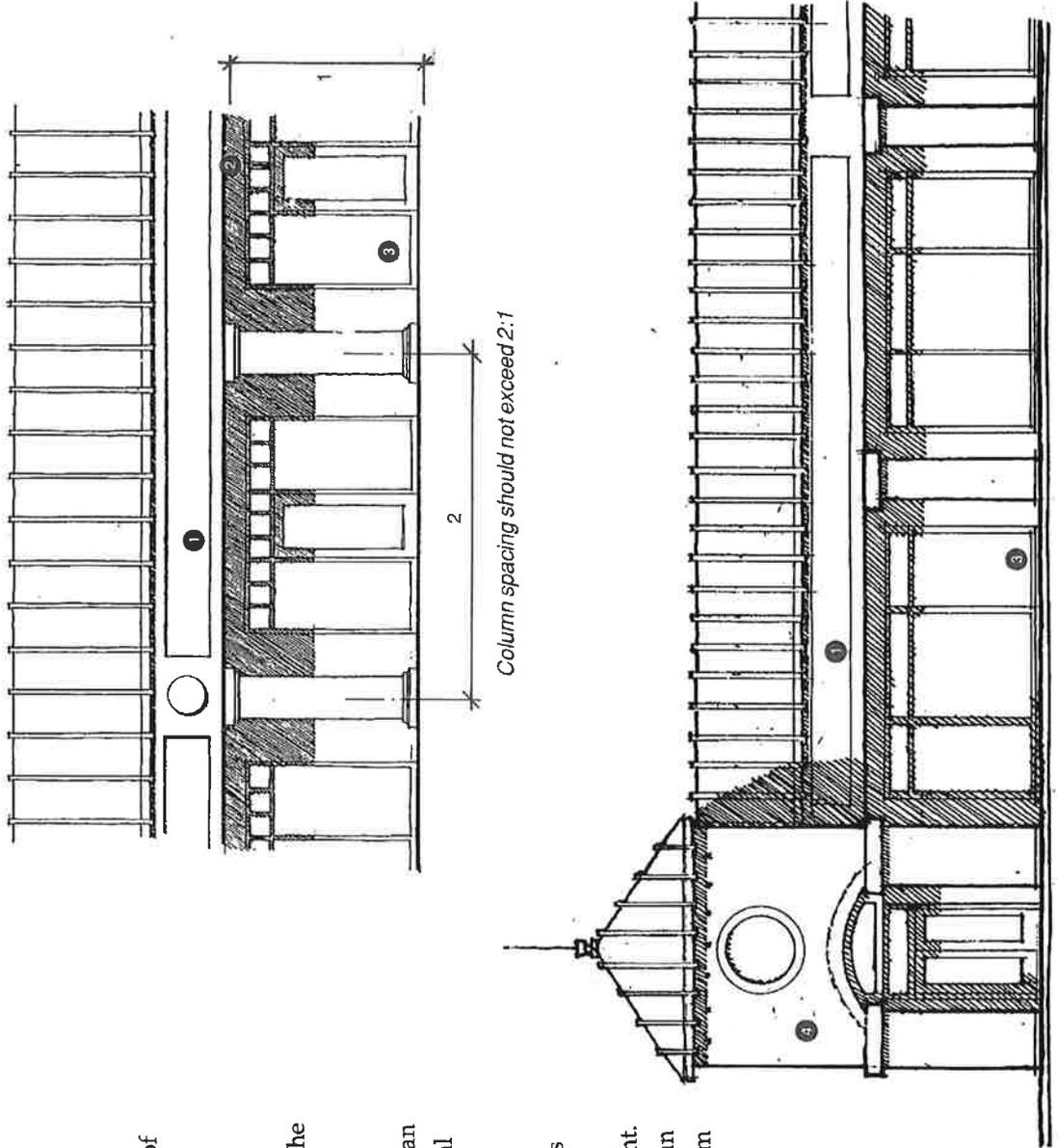


DESIGN ELEMENTS

The project designer shall give strong consideration to the following design elements, which are considered critical to the overall design continuity and quality of the area.

COLONNADES / ARCADES

Whether the colonnade is integrated into the storefront design and enclosed with glass, or whether it is open-air, the rhythm and proportion of columns and their spacing can add significantly to a project's architectural interest. The proportions of the elements are extremely important in creating a successful design. A basic rule of thumb is that the column spacing center to center should not exceed twice the columns height. For office or motel designs in the Craftsman Theme this element may even take the form of a porch or veranda.

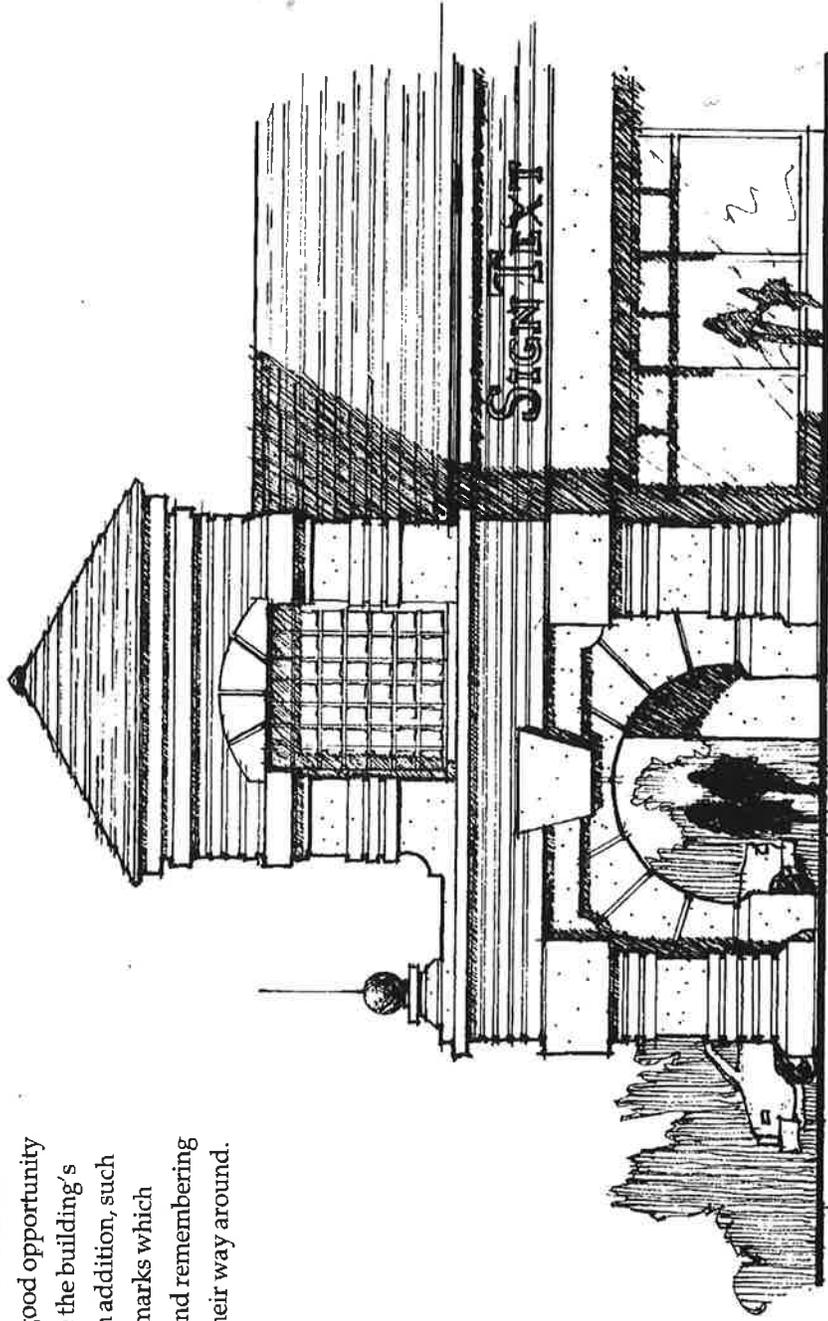


1. Sign Area
2. Top of Colonnade Opening
3. Colonnade with Storefront Beyond
4. "Terminus" Element



TERMINUS

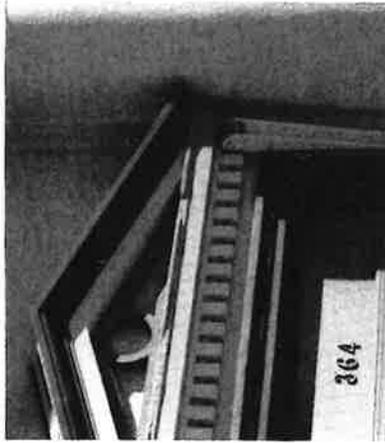
A strong architectural element, or “terminus”, will add punctuation or emphasis at the end of a long colonnade or storefront area. Such features offer a good opportunity for the designer to celebrate the building’s design theme and details. In addition, such elements act as visible landmarks which assist users in recognizing and remembering the project and in finding their way around.



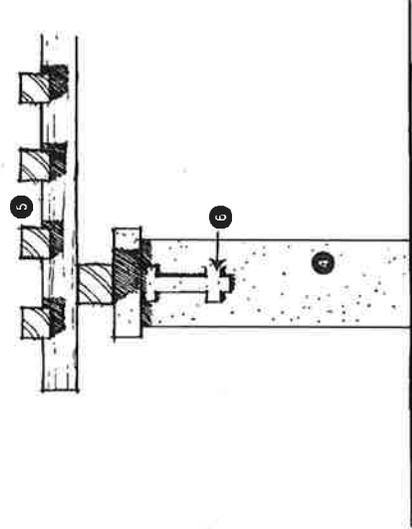
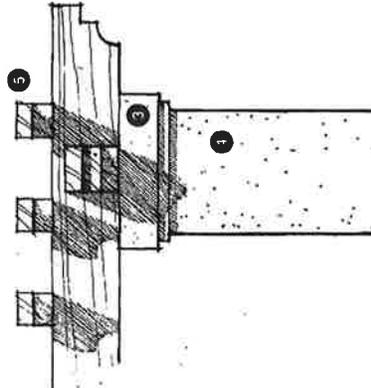
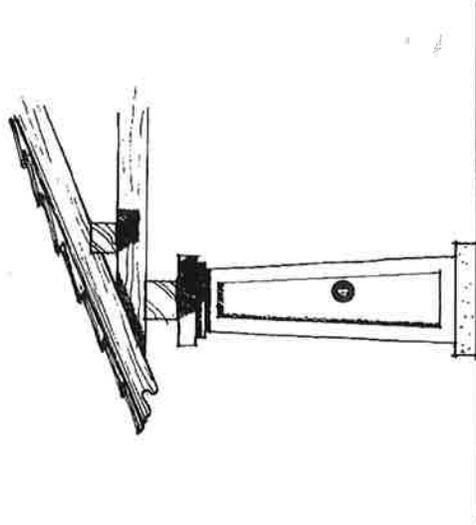
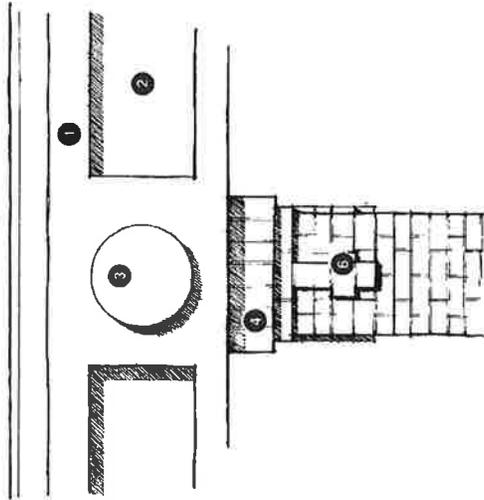


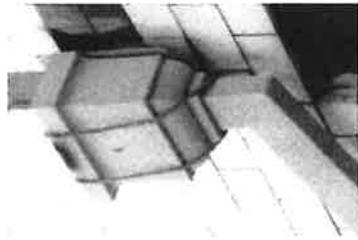
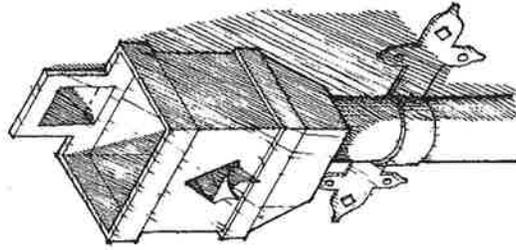
COLUMNS AND DETAILS

Both themes in this guideline offer a rich source of material for detailing. Column capitals and bases, parapets, scuppers and downspouts, lighting fixtures, and signage all represent an opportunity to express the architectural celebration of these themes. Again, proper proportioning is critical to a successful design.



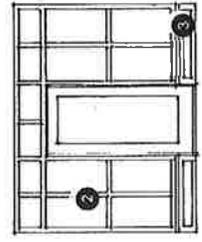
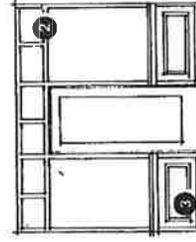
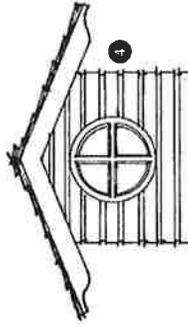
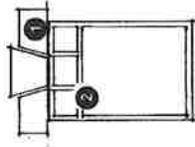
- 1. Building Fascia
- 2. Sign Area
- 3. Stucco Detail
- 4. Column Element
- 5. Trellis with Shaped Beams
- 6. Detail Relief





WINDOWS AND STOREFRONTS

Window details are important in both themes. Like columns in arcades, windows and other openings can be used to establish rhythms and patterns which add much to the character of a structure. Recessing important windows or other openings (such as service doors) can create a sense of thick and substantial walls and create drama with shadow lines. Storefront systems can take on a variety of patterns. Openings can be divided by mullions, filled with detailed wood panels (as in a "bulkhead"), to help create the image of early twentieth century shops.



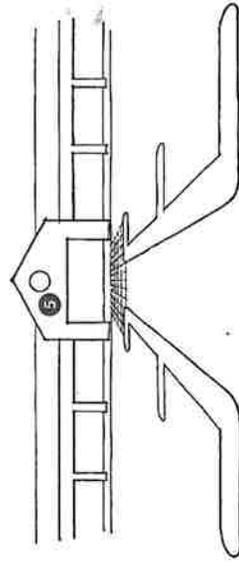
- 1. Lintel Detail
- 2. Mullion
- 3. Wood Panel "Bulkhead"
- 4. Dormer Window



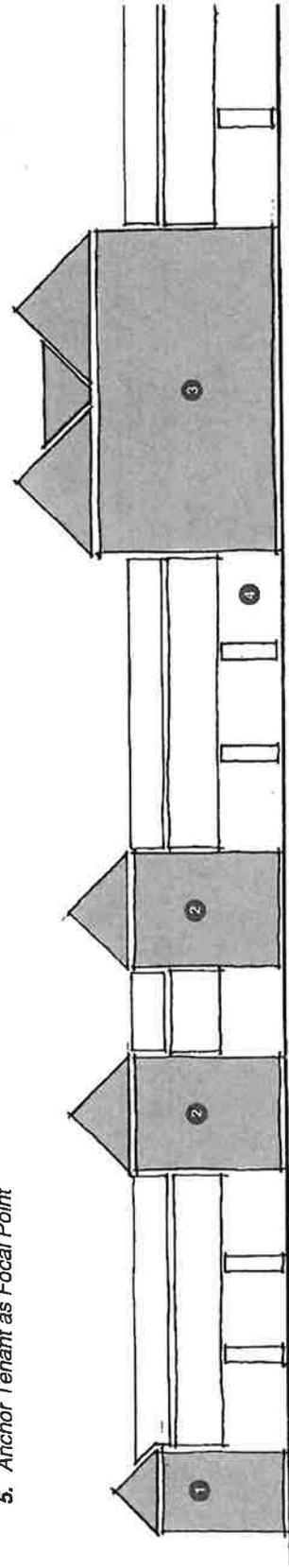
ROOFS AND ROOFLINES

An interesting, varied and “playful” roofline is the primary objective of this element. This can be accomplished by a variety of means including: providing for architectural relief and stronger expression at probable anchor tenant locations, separating buildings into two or more “parts” at logical points, allowing for pedestrian and/or vehicular pass-through and at the same time breaking or changing the roof line, selecting a roofing material with color and texture that will compliment the over-all design. In addition the following criteria shall apply:

1. Mixing sloped roof forms and parapets can be an effective way of breaking the roof line, creating the illusion of a larger or more complex building form and avoiding the common impression of the mansard box and storefront. Grand or over-sized parapets can be used to draw attention to anchor tenants, or to create a focal point (such as at the terminus to a main parking lot access drive).



1. *Terminus*
2. *Secondary Anchor Tenant*
3. *Primary Anchor Tenant*
4. *Colonade*
5. *Anchor Tenant as Focal Point*



Building mass can express a hierarchy of users

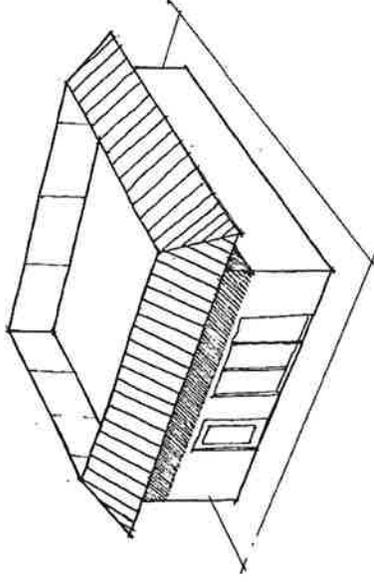


2. Roof mounted mechanical equipment must be screened from public view. Parapet and roof screening will be permitted only where they offer a logical and integrated extension of the building design. No Parapet or roof screen is to appear as being "tacked-on". For this reason, mechanical equipment must be considered early in the design process - not as an afterthought.

3. Roof vents, where visible from public view, shall be finished to blend into the surrounding material color.

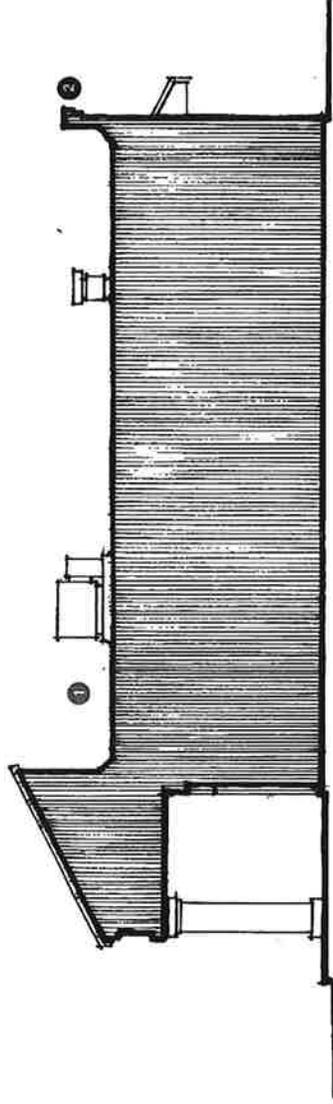
4. Mansard roofs will be permitted only when they are integrated into the over-all design. The "mansard" condition should not be readily apparent from the street level, but rather should be used to create an effective illusion of a full roof beyond. All too often mansards appear to be "hats" plopped onto flat buildings to hide the flat roof. It is this condition that the guidelines seek to avoid.

1. Mechanical Equipment is Well Screened by Building Form
2. Rear Parapet Adequate to Screen Venting Devices.



UNACCEPTABLE USE

The appearance of a mansard "Hat" is to be avoided.



Architectural screening of roof equipment is integral to the design



AWNINGS

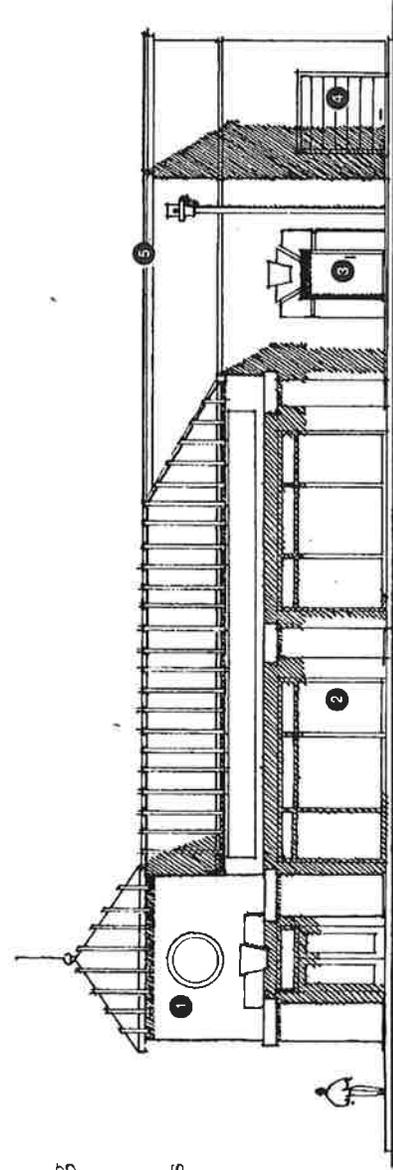
Awnings will be permitted where they represent an appropriate continuation of the building design. There are a variety of awning types and styles available, and each proposal will be reviewed by the Planning Commission for appropriateness. However, since awnings have a relatively short "life-span", the building design should appear complete without awnings in the event that the awnings are removed and are not replaced at some point in the future.



1. Terminus
2. Colonnade
3. Service Door Visible from Public Area
4. Service Door well Screened by Building Form
5. Continuous Cornice Detail

SIDE AND REAR ELEVATIONS

Design treatment on side and rear of buildings should represent a continuation of the basic design character of the building front. Changes in building materials and shifts in architectural emphasis will be done with particular care paid to sight lines from areas of high public use, both on and off the project site. Also refer to site development standards.



Side Elevation



EXTERIOR MATERIALS AND COLORS

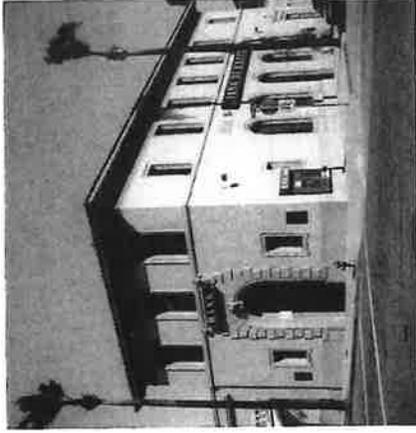
Exterior materials shall be compatible with the style chosen and should be selected with quality and long-term durability in mind. Materials such as wood or hard-board siding should be finished and sealed in a manner consistent with this objective and the manufacturer's recommended specifications. The following is a list of exterior finish materials that should be considered in the project design:

Walls: wood siding, brick, stone, stucco, cut shingle or shake (fire treated).

Roof: concrete tile, batten or standing seam metal, heavy textured composition shingle, cut shingle or shake (fire treated).

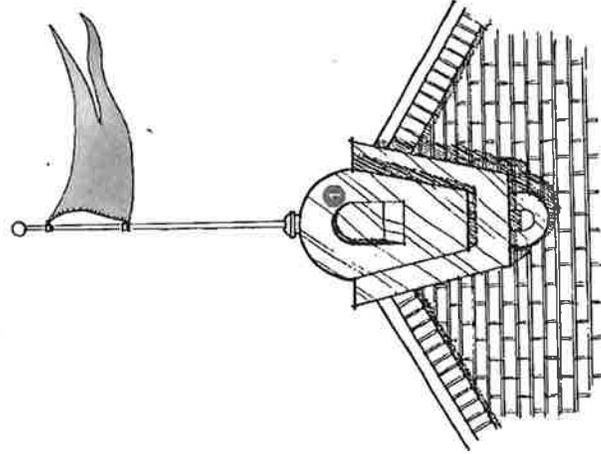
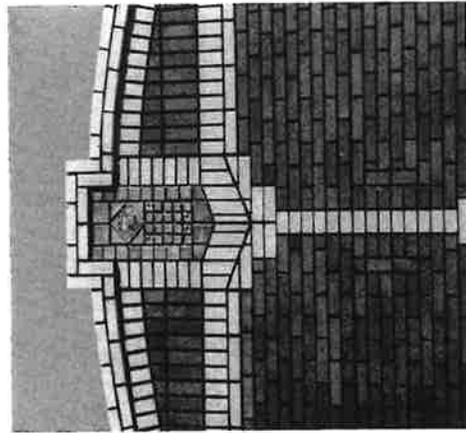
Detail: wood, copper, galvanized and painted metals, cast iron, aluminum with baked enamel finish, glazed tile.

1. Formed Sheet Metal Detail



Color design should be coordinated to integrate all elements of the project. Colors should be used to emphasize the details and features of the design, complementing natural material colors with applied color (paint or stain), and the colors of other features such as signage awnings, light fixtures, etc..

A thorough and complete materials / color board shall be submitted with project application and drawings. The Planning Commission shall review each proposal for appropriateness in its given context.





S I T E D E V E L O P M E N T

All applicable standards and requirements of the City of Fillmore Zoning Ordinance shall apply to development in the guidelines area. In addition, the following requirements shall apply:

BUILDING CONFIGURATION

The site and building should be planned to create site lines that are attractive from the street as well as from within the project boundaries. Visual interest can be created by allowing the building orientation to veer from street lines, rather than strictly paralleling them. Further, allowing the building forms to shift in and out may also create interesting exterior spaces while effectively accenting unique tenants. Buildings which create interesting spaces for people will tend to attract people and therefore contribute to the success of the building's tenants.

HARDSCAPING

"Hardscape" outdoor space should be considered an integral part of projects designed within the guidelines area. The intent is to provide attractive and welcoming areas for customers and employees of the businesses in these developments. These spaces should be designed in accordance with the following criteria:

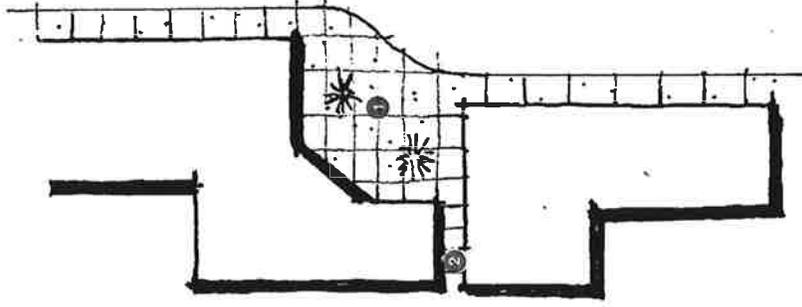
1. Each project over 5,000 square feet in floor area should provide a "hardscape amenity" in the form of patio or plaza space equal to 1/50th of the total building floor area. This area should be designed in a manner complementary to the project, and should be provided with fixtures and amenities in keeping with the design theme.

2. The hardscape areas should be located such that users of the building(s) are not required to cross parking areas to gain access to them.

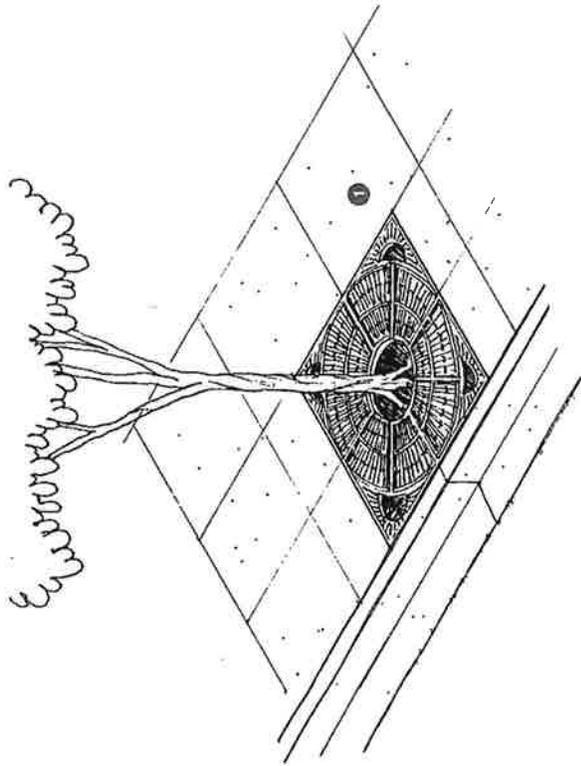
3. In some cases it may be appropriate that the hardscape area not be concentrated into one space, but be distributed throughout the project in a linear fashion or in the form of several small plaza spaces.

4. These spaces should be well lit in a manner consistent with the design theme, and should be provided with seating at the rate of one 6 ft. bench, or 6 lineal feet of seating, per every 5,000 square feet of building floor area.

5. Parking lot layout and lighting shall conform to zoning ordinance requirements.

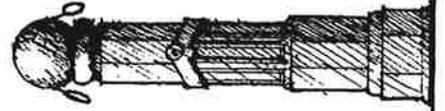
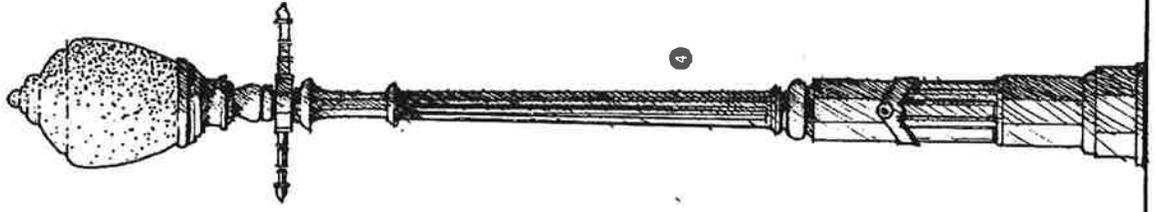
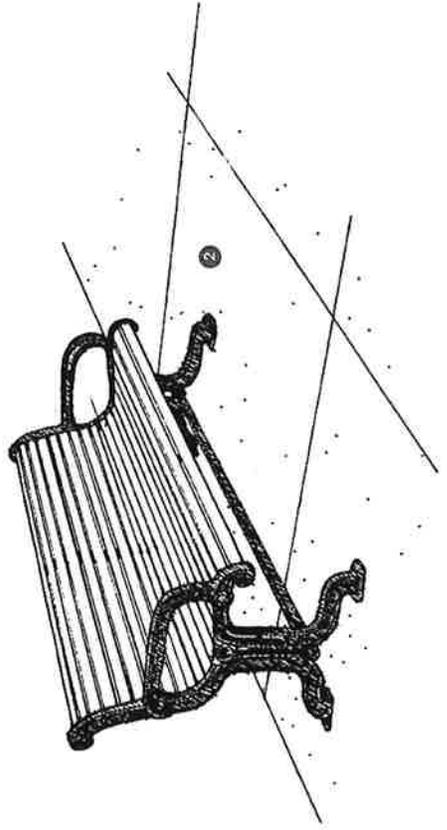


1. Plaza or Court Space
2. Pedestrian Pass-Through



Typical traditional site work details to be considered in projects:

- 1. Ornamental Tree Grate
- 2. Wood and Iron Bench
- 3. Traditional Iron Bollard
- 4. Traditional Iron Street Lamp.





LANDSCAPING

Landscaping and irrigation shall be required of all projects covered by these guidelines. Landscape plans shall be prepared by a licensed landscape architect registered in the state of California, and shall conform to the requirements as set forth below:

1. Landscaping design should be attractive, colorful, well coordinated with the architectural design, and give the general impression of abundant planting. Planting should be used where it will have the most impact aesthetically, not simply to satisfy area or quantity requirements.

2. On-site landscaping shall be provided to meet a minimum area of 12% of the project site. Landscaped areas shall be watered by use of an automated irrigation system and shall be bounded by concrete curbing having a minimum height of six inches.

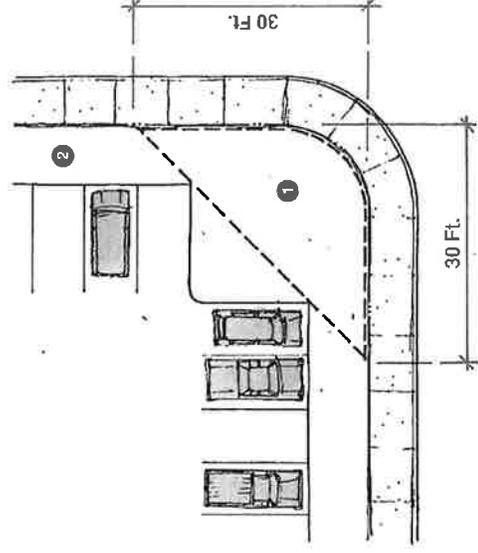
3. Ground cover shall be evergreen and colorful, drought tolerant species and planted from flats at 12" on center (maximum) spacing. The ground cover shall be of a type capable of covering the entire planted area within six months from time of planting.

4. Shrubs and vines shall be 5 gallon size minimum and of a drought tolerant species.

5. Street trees shall be as specified by the planning department and shall be 24" box size (minimum). At least fifty percent of all other trees in the project shall be 24" box size (minimum).

6. All equipment enclosures, transformers, trash enclosures, backflow devices and irrigation controllers shall be screened from view with shrubs and/or vines, 5 gallon size minimum.

7. Projects located at the corner of a street intersection shall maintain a thirty foot triangular area at the corner of the lot with maximum plant material heights of no more than 36" above street elevation at the curb, with the exception of trees, which shall be trimmed and maintained so that no branches extend lower than 6 feet above curb level. This is to assure that landscaping materials will not obscure driver and pedestrian visibility at these intersections.

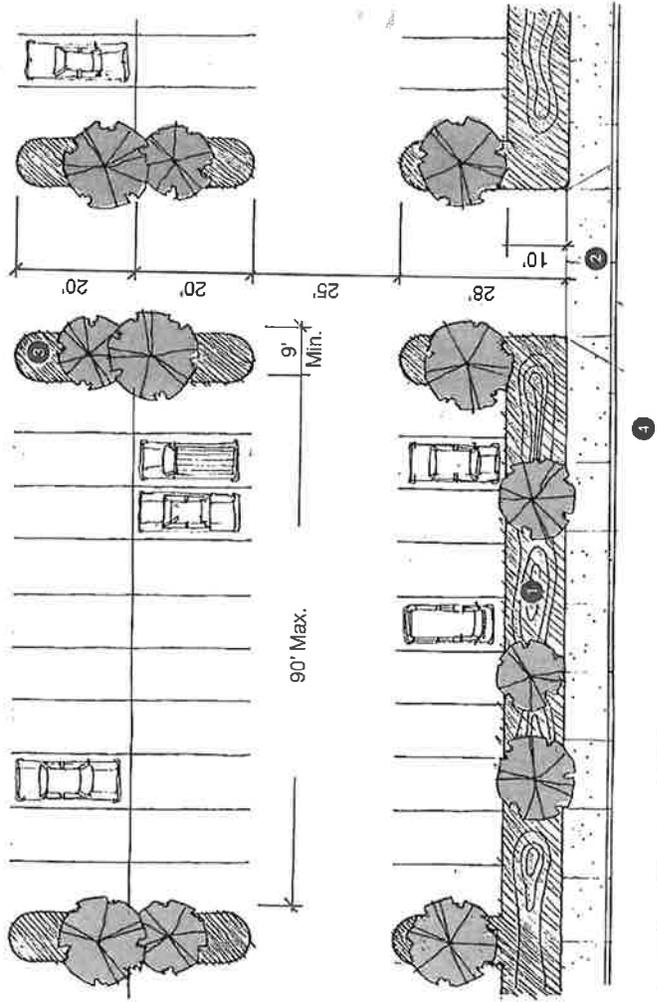


1. Visibility Triangle
2. Typical Landscape Area

Typical corner landscaping at street intersection



8. All projects abutting a public street shall have a continuous landscaped area of ten feet in depth (minimum) across the project frontage, with openings for pedestrian and vehicle access in accordance with city standards. This landscape area shall incorporate variable height planting (shrubs, ground cover, and trees) in combination with berms and mounding to create a visual screen averaging 36 inches in height (above curb) between the street and all parking areas.
9. Individual landscape areas shall have a minimum area of twenty square feet and a minimum cross dimension of four feet as measured clear face to face from inside of curbs.
10. Parking lot landscaping shall account for approximately 1/3 of the total landscape area and shall be evenly distributed throughout the parking area. Planter areas with trees shall be installed at ten car intervals for single row parking (twenty car intervals for double row parking).



Typical Parking Lot Landscaping

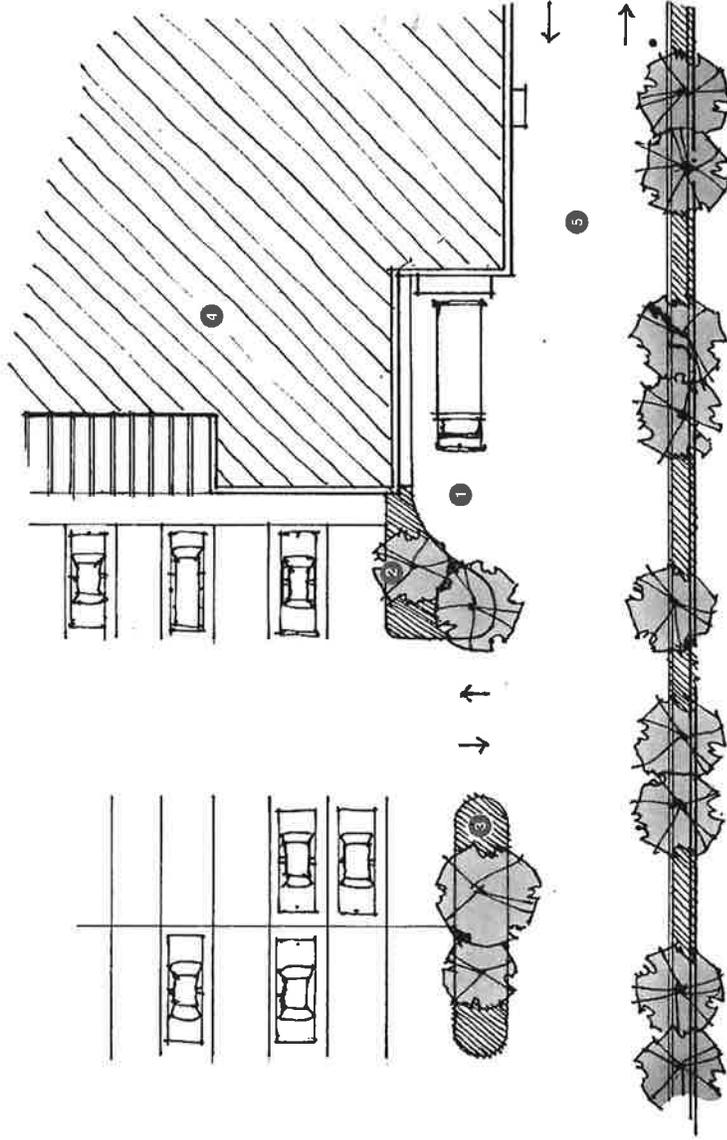
1. Variable Height Planting
2. Vehicle Access
3. Typical Parking Lot Planter
4. Public Street



DELIVERY AND SERVICE AREAS

Delivery and service areas are to be given specific consideration in the site and building design as follows:

1. Visibility of delivery and service areas from public streets and adjacent properties is to be minimized. Screening is to be accomplished by building configuration where possible. Where necessary, screening may be achieved by the use of architecturally integrated walls in combination with appropriate landscaping.
2. Refuse enclosures are required and shall be provided with the same considerations as required for delivery and service areas. Refuse enclosures must be provided with closable, aesthetically compatible gates and separate pedestrian entryways.
3. All mechanical equipment shall be screened from public view. Such screening shall be achieved through architectural and/or site design elements consistent with materials, colors, and textures of the building.



1. Loading Area
2. Landscape Screen
3. Typical Landscape Planter
4. Building
5. Vehicle Service Access

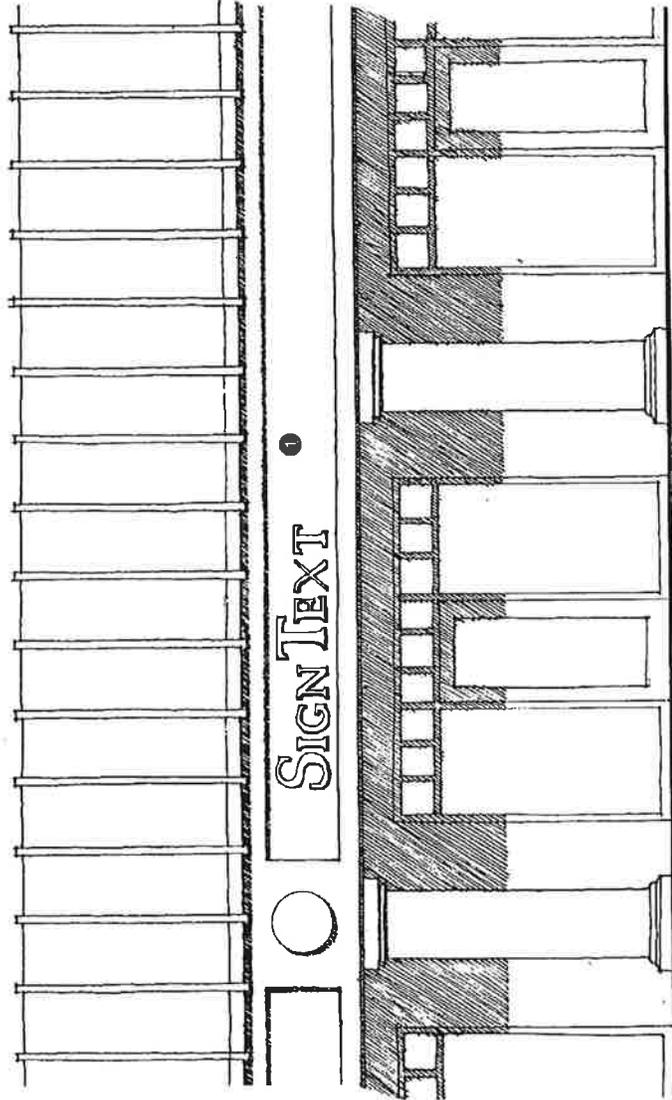


SIGN DESIGN STANDARDS

It is the intent of these guidelines to provide the project designer with practical alternatives in signing the project, while assuring that the end result will be high in quality, aesthetically pleasing and appropriate to the selected building style. All applicable standards and requirements of the City of Fillmore's Zoning Ordinance shall apply to development in the guidelines area. In addition, the following requirements shall apply:

SIGN PROGRAM

All proposed projects in the Guidelines Area shall include a "Project Sign Program" as a part of the application submittal package. This program shall clearly establish the criteria to be used by tenants in providing signage for their businesses. The sign program shall include a site plan and building elevations depicting probable locations of signs and listing them by type and approximate size, allowable materials, and method of illumination, if any.



1. Tenant Sign Design to be Compatible with Building Design and Colors



PERMITTED SIGN TYPES

Project Signs

These signs are to be designed in a manner consistent with the project proposal, utilizing similar colors, materials and architectural detail. All signs, especially project signs, offer an opportunity to express and celebrate the building theme.

1. Monument Signs
2. Free-Standing Signs (where permitted by ordinance and deemed appropriate by the Planning Commission).

Tenant Primary Signs

1. Individually Mounted "Channel Letters"

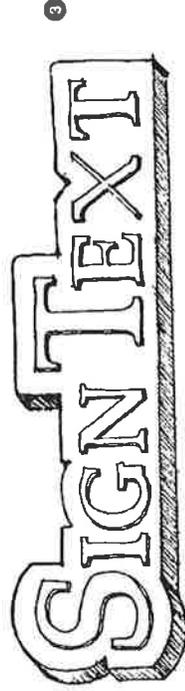
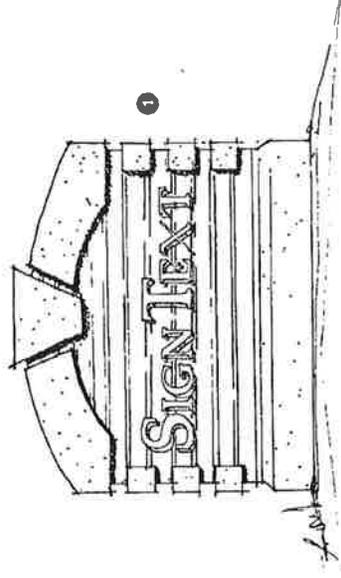
Note: Although this is an aesthetically superior sign type, consideration should be given to its use in areas where required, conduit holes in finish materials may leave permanent scars if the sign is removed or altered. This is not a problem if the sign is to be non-illuminated.

1. Monument Sign
2. Channel Letters
3. Shaped or Scribed "Can" Sign

2. Shaped or Scribed "Can" Signs

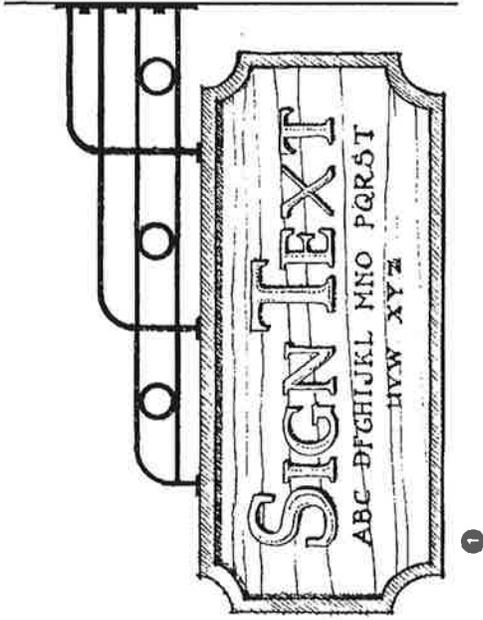
The shaped or scribed "Can" sign is one in which the can generally follows the outline of the word or words being signed. It therefore has some of the visual interest of "channel letter" signs, but with less cost and fewer conduit holes in the building surface.

Note: Exposed neon lights are not prohibited, but should be used sparingly and must be identified in the sign program.



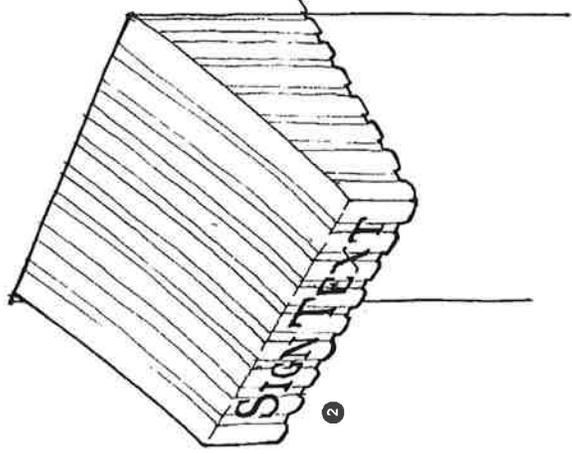


3. Carved or Cast Letters
Such signage may be mounted directly to specified locations on the building, or they may be attached by means of decorative brackets or hangers as illustrated here.



1

4. Awning-Integral Signs
Awnings are available in a wide variety of materials and colors, with several options for incorporating signage and illumination. Awning-integral signs will be permitted where the Planning Commissions deems them appropriate to the proposed building design. Detailed drawings clearly depicting the proposed signs must be submitted along with material samples at the time the project application is submitted.



2

1. Wall Mounted Bracket Sign
2. Awning Integral Sign



Projects may include some signage inspired by the older style signs found on Central Avenue. Such sign types should be used sparingly, to identify a unique tenant, such as a restaurant



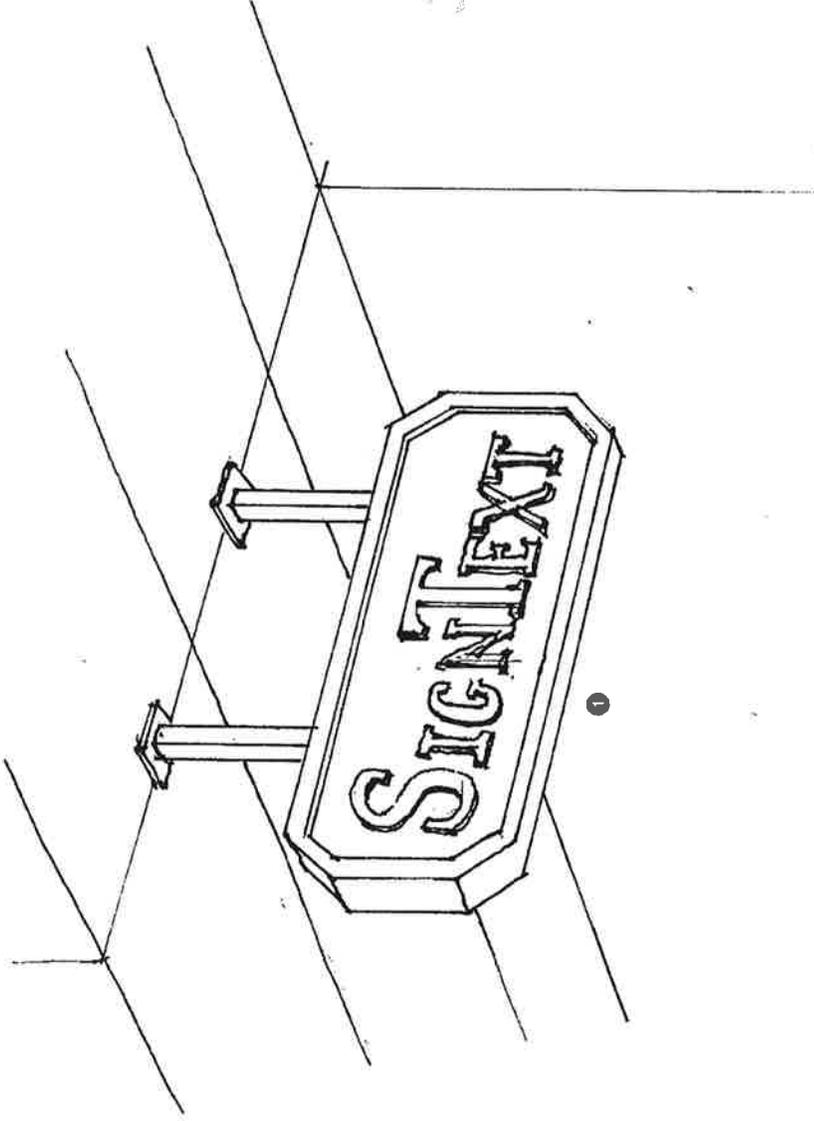
Tenant Secondary Signs:

1. Under-Canopy Sign

Projects which include exterior arcaded areas may incorporate under-canopy signs perpendicular to building front to assist pedestrians in locating businesses. These signs shall be decoratively designed in keeping with character of the project design and shall not exceed three square feet in area on one side.

2. Rear or Loading Entrance Sign

Where tenant spaces are provided with rear or loading entrances, a sign may be provided for the purpose of identifying the access for employees and delivery vehicles. The sign shall not exceed two square feet in area, and shall be mounted flush to the building surface, no higher than seven feet above the building floor line.



1. Under Canopy Sign

Attachment #3

IV. GOALS and POLICIES

According to the State General Plan Guidelines, the General Plan is a “collection of development policies.” These policies guide growth within the City limits and the City’s Sphere of Influence. The development that is allowed by the Land Use Element is to be directed by the goals, objectives, and policies in this section of the Element, and implemented through the related implementation measures contained in the following section.

In this element, GOALS are statements that provide direction and state the desired end condition. POLICIES are specific statements that guide decision-making. They indicate a clear commitment by the City and generally serve as mandatory criteria.

Goals



Urban Form:

1. Maintain the City’s small-town, rural character in order to enhance the physical, emotional and mental well-being of the City’s residents.
2. Preserve Fillmore’s unique physical and social character by requiring high quality urban design within development to promote architectural integrity and enhance the overall appearance of the community.
3. Ensure that proposed land uses are consistent with the desires of the community.
4. Encourage infill and mixed-use development as a priority within the existing City limits, where appropriate and feasible.
5. Apply the traditional style and character of the City’s older commercial and residential areas to new development, where appropriate.
6. Create opportunities for quality residential, commercial, business park, and recreational development in such a manner as to ensure orderly development.
7. Prohibit urban expansion within the unmitigated floodway of the Santa Clara River, Sespe Creek and Pole Creek.
8. Encourage the development of new neighborhood schools.

The Environment:

9. Protect environmentally sensitive areas.
10. Protect the environmental and cultural resources of the City and surrounding area for the long-range health, safety and general welfare of all citizens.
11. Promote the preservation and wise use of the region’s natural and agricultural resources.

12. Maintain an acceptable noise environment throughout the community through protection of noise-sensitive areas from the harmful effects of noise pollution.
13. Preserve the existing nighttime environment by limiting the illumination of areas surrounding development.
14. Minimize the risk of exposure to the public from natural and man-made hazards.
15. Promote energy and water conservation activities throughout the community.

Housing and Community Development:

16. Address the housing and community development needs of all of the City's economic and age groups, as well as the needs of its disabled.
17. Ensure that residential areas are developed and redeveloped to be healthful, safe, and attractive neighborhoods, served by adequate open space, recreational and community facilities.
18. Manage population growth so as to enhance the economic, social and physical environment of the City.
19. Foster civic pride by providing opportunities for volunteerism and citizen involvement to enhance community services, including education and recreation.
20. Provide adequate public services to serve present and future residents.
21. Provide park and recreation facilities that satisfy the diverse recreational needs of all segments of the population, and all age groups.

Economic Growth:

22. Encourage a balanced community with a variety of housing, economic activities, and employment investment opportunities.
23. Maintain a balanced and diversified economy, emphasizing the agriculture industry, locally-owned business and industry, and tourism, which broadens the economic base of the community.
24. Encourage the development of industrial areas that provide employment opportunities by attracting clean, business park style development.
25. Provide a variety of retail opportunities, including downtown and highway commercial development.

Circulation:

26. Provide for the efficient and safe movement of people, goods and services within and through the City.
27. Develop and maintain an interconnected network of roadways, bikeways, pedestrian paths, and rail lines to accommodate the travel, business and recreation needs of all residents.
28. Encourage urban development that incorporates elements of traditional town design, emphasizing alternative transportation modes, including walking, bicycling, and transit use.
29. Ensure that the City's commercial areas are convenient for pedestrian and vehicular access.
30. Pursue regional truck routes that provide alternate access around Fillmore.



Policies

Urban Form:

Citywide

LU-1 Provide land area to accommodate housing units that meet the diverse economic and social needs of the residents, locating development to:

- *Retain the scale and character of existing residential neighborhoods.*
- *Facilitate the upgrade of declining and mixed density residential neighborhoods.*
- *Provide for high-end housing, which is not currently provided for.*

LU-2 Provide incentives for development that will:

- *Provide distinctive architectural design and site planning.*
- *Incorporate streetscape and other public urban design amenities that contribute to a high quality image and benefit the community.*

LU-3 Allow for infill of vacant lots and reuse of underdeveloped property at the neighborhood's prevailing scale and character.

LU-4 Preserve the traditional style and the historic appearance and character of the City's older commercial and residential neighborhoods.

LU-5 Infill development shall be with design features that complement surrounding structures.

LU-6 Require that techniques be used to avoid “box-like” commercial structures, including, but not limited to: differentiation of facades and elevations, articulation of building details (roof, columns, beams, balconies, arcades, trellises, recessed windows, etc.).

LU-7 Encourage neighborhood designs whose appearance is not dominated by the automobile, where front porches, homes fronting parks and parkways are encouraged, and garage-dominated streets are discouraged.

LU-8 Require that non-residential development be designed to orient outward to pedestrian sidewalks and public streets.

Highway 126



LU-9 Develop signs, monuments or other physical features that announce the entrance to the City and/or the downtown.

LU-10 Prohibit the development of new commercial billboards in the portion of the Highway 126 corridor within City limits.

LU-11 Encourage new business and commercial development that reflects the traditional style and historic appearance of the City’s older architecture.

Along the Railroad Corridor

LU-12 Permit the development of railroad-related uses and railroad-dependent uses in the railroad corridor.

LU-13 Encourage land uses on vacant and underdeveloped land adjacent to the railroad corridor that is compatible with the railroad as well as adjacent established conforming land uses.

LU-14 Require that new development be compatible with and sensitive to the railroad corridor.

LU-15 Maximize the use of any excess right-of-way land to enhance the corridor.

LU-16 Improve the visual appearance of the railroad right-of-way through the City.

LU-17 Encourage development along the railroad corridor that incorporates a bicycle and pedestrian trail.

The Environment:

Citywide

LU-18 Preserve viable agriculture and prime agricultural lands as a greenbelt and buffer outside the City’s Sphere of Influence.

- LU-19 The creation of incompatible land uses or land uses that are incompatible with sensitive environmental areas should be avoided.
- LU-20 Wherever residential uses abut either industrially-designated lands and/or Highway 126, the potential conflicts between these land uses shall be mitigated through the establishment of landscaped buffers.
- LU-21 Wherever urban uses are proposed adjacent to significant habitats, sensitive uses shall be mitigated through the establishment of fire retardant and native vegetative buffers, consistent with the requirements of appropriate state and federal regulatory agencies.
- LU-22 Require that all commercial development provide buffers with adjacent residential uses or residentially zoned property, including but not limited to: decorative walls, landscaped setbacks, restricted vehicular access, proper siting and screening of trash and service areas and control of lighting and noise.
- LU-23 Scenic views and vistas, tree-lined streets, open spaces, natural areas, ridgelines, viewsheds, and landforms should be preserved.
- LU-24 Preserve important natural features, such as barrancas, tree rows, wetlands, ridgelines, and wildlife movement corridors.

Hillside Areas

- LU-25 Assure that development in the city's hillside areas occurs in a manner that protects the hillside's natural and topographic character and identity, environmental sensitivities, aesthetic qualities and the public health, safety and welfare and adheres to development standards.
- LU-26 Ensure that hillside development does not lead to soil erosion, mass grading, severe cutting or scarring and/or large removals of vegetation.

Housing and Community Development:

- LU-27 Development and expansion shall pay for its fair share of the costs of its public service and infrastructure needs and shall pay the costs of needed utility services.
- LU-28 Development shall be designed so that it can be efficiently and economically served by City services.
- LU-29 Review all new development and annexation proposals to ensure that the City can provide sufficient water production, treatment, and storage capacity to meet acceptable standards.
- LU-30 Review and update impact fees and/or development agreements to assess land development projects for the costs of public facilities, utilities, and infrastructure needed

to serve such projects, including but not limited to the following: fire, police, roads, sewers, flood control, recreation, schools and water.

- LU-31 New infrastructure shall be sized to support the projected population growth of the community.
- LU-32 Community parks should be developed to serve new and existing residential development.
- LU-33 Accommodate recreational demand for uses such as playing fields, skating, teen centers, performing arts, basketball, tennis, swimming and various trail systems, including equestrian and bicycle trails.
- LU-34 Encourage the continuation and enhancement of schools, school-related uses and school grounds within the city.
- LU-35 Encourage Historic Preservation as a valuable tool to retain the City's heritage.
- LU-36 Provide assistance through loan and grant programs from the redevelopment agency for private residential uses to enhance exterior of properties for community beautification, and bring properties up to health and safety standards.

Economic Growth:

Citywide

- LU-37 Provide incentives for development that will contribute significant social and economic benefits to the community.
- LU-38 Emphasize the attraction, retention, and promotion of businesses.
- LU-39 Support regional agricultural businesses for uses such as farm tours, farmers markets, high scale fruit stands, and flower growers outlets.
- LU-40 Promote businesses through a positive community image, fostered by a viable work force, quality housing, and educational opportunities.
- LU-41 Promote festivals to attract visitors to the City.
- LU-42 Encourage commercial and business development along Highway 126.

Central Business District

- LU-43 Implement and encourage redevelopment programs that will stimulate the economic health of the Central Business District, by clustering specialty retail and service activities in this area.

LU-44 Encourage mixed uses within the Central Business District.

LU-45 Encourage the development of underutilized sites.

LU-46 Encourage tourist-related uses and community-oriented commercial activities such as festivals, cafes, vendors and sidewalk sales in the Central Business District.

Circulation:

LU-47 Use the railroad right-of-way as a multi-purpose corridor for transportation and recreation.

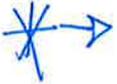
LU-48 Require the dedication and development of pedestrian/bicycle trail linkages throughout the City.

LU-49 Encourage and promote alternative modes of transportation.

LU-50 Represent the City through Caltrans and other transportation agencies to establish alternative regional truck routes to bypass Fillmore, which may include, but not be limited to, Highways 5 and 101.

V. IMPLEMENTATION MEASURES

This section of the Land Use Element indicates the actions and programs that shall be carried out by the City of Fillmore to implement the land use goals and policies. These implementation measures, together with the policies, establish and guide the City's annual budget process and day-to-day decision-making so there is continuing process toward attainment of the goals. The implementation measures presented below are categorized by the same topics as the goals and policies in the preceding section.



Urban Form:

1. Adopt all land use designations and overlay districts, with accompanying standards, as included in this Land Use Element.
2. Review and update as necessary the Land Use Element to evaluate land use trends and ensure that a balance is maintained between employment and housing opportunities, and to ensure the availability of sufficient municipal revenues to maintain adequate levels of public services.
3. Adopt an ordinance to strengthen Fillmore's existing greenbelt agreement with Santa Paula.
4. Maintain the greenbelt ordinance with Ventura County for the Santa Clara River Valley between Fillmore and the Ventura/Los Angeles County line.
5. Amend the Sphere of Influence to include 217 acres along east of Pole Creek, known as the PanAmSat site, as depicted in this Land Use Element (Expansion Area 1).
6. Amend the Sphere of Influence to include 65 acres south of the existing sphere to be included in the Southeast Specific Plan, as shown in this Land Use Element.
7. Amend the Sphere of Influence to include 199 acres, known as the Johanson Site, north of State Route 126 and east of the existing Sphere of Influence limit line, as shown in this Land Use Element (Expansion Area 2).
8. Annex lands as development proposals are reviewed and approved by the City.
9. Require the dedication and development of trails to and along the Santa Clara River and Sespe Creek at the time of annexation of areas adjacent to these drainage corridors.
10. Establish an Urban Growth Boundary, beyond which no further expansion of the City's Sphere of Influence could occur.
12. The following Development Standards for the **Southeast Area** shall be implemented through a Specific Plan and subsequent development approvals:

- No more than 1,000 homes may be constructed within the Specific Plan area.
- The area shall include a new elementary school **of not less than 10 acres**.
- The area shall include community parkland at least **20 acres** in size.
- The area must incorporate pedestrian and bicycle trail facilities, consistent with the intent of the proposed Santa Paula Branch Line Trail.
- The area shall include a neighborhood-serving commercial component.
- The area must include flood protection, as determined by Ventura County Flood Control District **and Federal Emergency Management Agency (FEMA)**. Levees shall be designed to maintain as natural an appearance as possible.
- Development shall be designed and sited to maintain views and vistas and to protect natural habitat.
- Pole Creek shall be buffered from development **and shall be** consistent with the requirements of appropriate state and federal regulatory agencies.
- Require the use of architectural design themes that complement and do not dominate the setting.
- Use building materials, colors, and forms that contribute to a neighborhood character.
- Clustering of development shall be promoted to protect sensitive habitat **areas**.
- Require a geotechnical/soils study to determine the potential for high groundwater and unstable soils.
- Require circulation system to tie in with the existing circulation system.
- Require new lighting that is part of any proposed development to be oriented away from sensitive uses and shielded to the extent possible to minimize glare and spill over.
- Require rear access to the existing mobile home park.

13. The following Development Standards for the **Johanson** expansion areas shall be implemented for individual developments within the area:

- Development shall be designed and sited to maintain the rural ranch estate character by including significant open spaces, views and natural habitat.
- Require the use of architectural design themes that complement and do not dominate the setting.
- Use building materials, colors, and forms that blend into the environment and contribute to a neighborhood character.
- Clustering of development shall be promoted, particularly to avoid development in steeply sloped areas.
- Oil seeps shall be contained and buffered.
- Require a geologic study for all development sites and roadways to address slope stability, faults and landslides.
- Locate building pads and develop the sites and roadways with minimized grading and reduced amounts of cut and fill slopes.
- Require the inclusion of drainage and flood control improvements designed to be natural in appearance.
- Require the use of fire retardant landscaping, adequate clearings, and fire retardant/fire proof building materials.
- Require circulation system to tie in with the existing circulation system.

- *Avoid ridgeline development on prominent ridgelines.*
- *Require new lighting that is part of any proposed development to be oriented away from sensitive uses, and shielded to the extent possible to minimize glare and spill over.*

14. New development shall be consistent with the scale of the property in question; for example, small lots should have small buildings.
15. Require a fiscal impact analysis for all annexations contemplated.
16. Review and update impact fees and/or development agreements to assess land development projects for the costs of public facilities, utilities, and infrastructure needed to serve such projects, including but not limited to the following: fire, police, roads, sewers, flood control, recreation, schools, and water.
17. Establish maintenance assessment districts or other similar measures to recover the cost of services required by new land development projects.
18. Enter into land development agreements for major new projects to assure significant contributions toward meeting community needs.
19. Enhance the existing park in the downtown to serve as a community gathering and performing place.
20. Develop a street tree program to provide a master plan for street tree placement and replacement.
21. Establish a program to increase the number of buildings protected as historic structures.
22. Establish design standards for gateway areas and provide for design review and approval for new development and remodeling of existing buildings in these areas, including the western and eastern ends of SR 126, and SR 23.
23. Adopt a Hillside Development Ordinance, based on the principles outlined for the Hillside Overlay District.
24. Use the following design standards for review of proposed infill commercial and residential development:
 - *Architectural style should be compatible with the existing neighborhood style.*
 - *The size, scale, and bulk of new development should be comparable to existing neighboring buildings.*
 - *Avoid large expanses of building walls without architectural interest.*
25. Ensure that flood control projects are designed utilizing colors, materials, and other design features that allow the projects to blend into the surrounding environment.



26. Provide incentives for development on properties adjacent to the railroad corridor that will promote small businesses, retirement housing, high-density housing, visitor serving uses, and railroad-related uses.

The Environment:

27. Establish and enforce standards for property maintenance (debris and weed removal, storage of automobiles, storage of material, and removal of dilapidated buildings or structures).
28. Adopt attainable and enforceable land use, noise, and light standards that protect persons within the community from the effects of noise, light, and glare.
29. Review all development proposals adjacent to agriculture for impacts on agricultural land and crops.
30. Require all proposed development adjacent to agricultural uses to provide a buffer (setback, landscaping, erosion control measures recreational uses, street), or implement other methods that would effectively minimize impacts.

Housing and Community Development:

31. Adopt a new Growth Management Ordinance based on performance standards.
32. Analyze all new residential development for impacts from population growth on public services, infrastructure, and schools and environmentally sensitive areas.

Economic Growth:

33. Permit processes that fall under the jurisdiction of the City of Fillmore should be streamlined, while ensuring quality development.
34. City departments should coordinate their efforts to assist business and development.
35. Provide Redevelopment assistance to create buildings for new businesses.
36. The City should assist community organizations to identify, train, and place unemployed and under-employed residents within the City of Fillmore.
37. Coordinate with County and other State and Federal job training organizations to address the employment needs of new and existing businesses in Fillmore.
38. Determine what needs in the community are key factors in improving the economic base of the community, such as but not limited to housing, health care, water and wastewater facilities, public recreation, education, and public safety.

39. Develop a business retention and attraction program to minimize vacancies in commercial and industrial areas.
40. Locate or develop an appropriate space for festivals that provides a large vacant land area and sufficient area for parking and barriers to sound.
41. Fund an economic marketing strategy.
42. Promote Fillmore as a film making location by providing coordination between film studios, local businesses, and residents.
43. Promote Fillmore's railroad, historic downtown and agricultural attractions through intensive marketing.
44. Through public and private partnerships, develop attractions, including, but not limited to: a venue for farmers markets, arts and crafts fairs, artist showings, a citrus museum, farm tour, an antique airplane and car museum and a tourist trolley.
45. Encourage the formation of a partnership with the Fillmore Chamber of Commerce and the Heritage Valley Tourism Bureau.
46. Promote High School student apprentice programs with local businesses.
47. Provide incentives to assemble and attract larger commercial and industrial developments.

Circulation:

48. Continue to work with the Ventura County Transportation Commission to develop a bike/hike trail along the railroad right-of-way.

Public Services:

49. Implement a phased program to replace all substandard water mains, fire hydrants, and facilities related to the water conveyance system.
50. Upgrade water collection, storage, treatment, and conveyance systems in conjunction with development.
51. Establish and implement a long-term plan for recycling, with specific collection goals for each recyclable material category and a method to track quantities of materials. The goal should be a 50% waste stream diversion.
52. Assist the Fillmore Unified School District to develop cable television production program to be sponsored by the City in conjunction with the schools to produce and broadcast programs for students created by students.

53. Prepare a City video or slide show for use at schools, on City Hall tours, and as a promotional item.
54. Prepare an educational program for schools and city tours to promote a better understanding of City government.
55. Develop and promote a program to more effectively utilize volunteers.
56. Publish a newsletter to keep citizens informed of issues and events.
57. Establish an on-going business forum between city officials and city businesses to receive input on plans, policies, and budget items, possibly building collaborative agreements.
58. Continue to promote the Heritage Valley concept and promote tourist uses along the railroad.