



CITY OF FILLMORE DOWNTOWN SPECIFIC PLAN

PREPARED FOR THE CITY OF FILLMORE

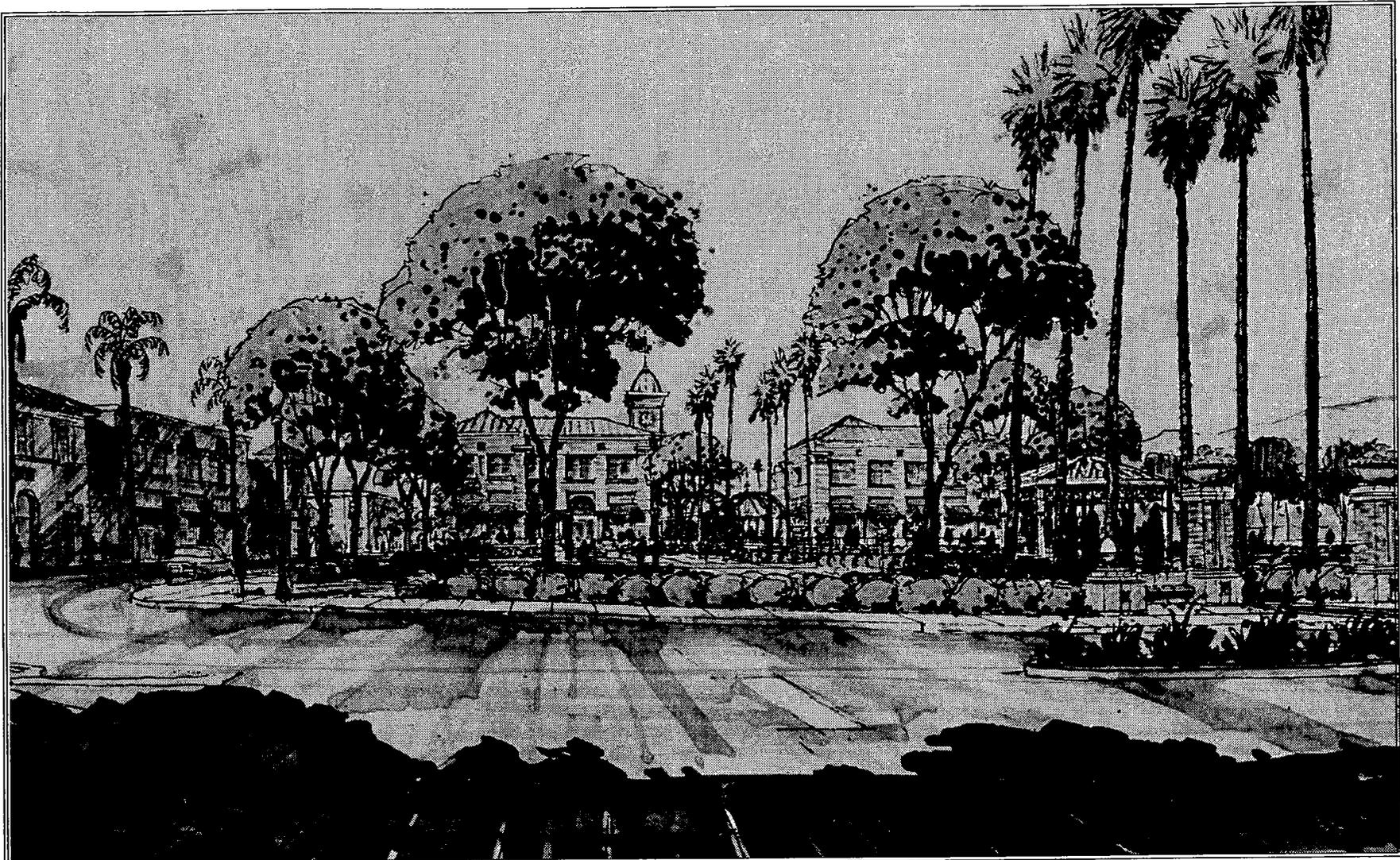
BY

MAINSTREET ARCHITECTS AND PLANNERS

STEPHANIE LAWSON CONSULTING

EPT LANDSCAPE ARCHITECTURE

MARCH 1994



**A VISION FOR DOWNTOWN FILLMORE:
A NEW TOWN SQUARE AT CENTRAL PARK**



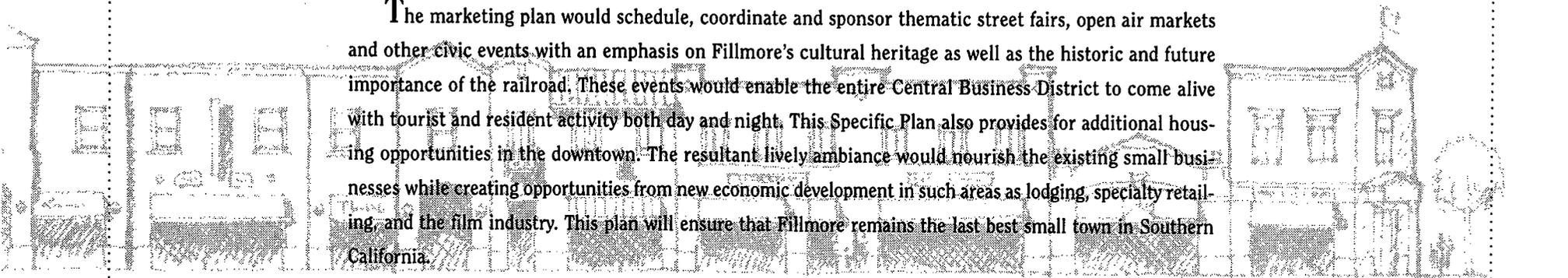
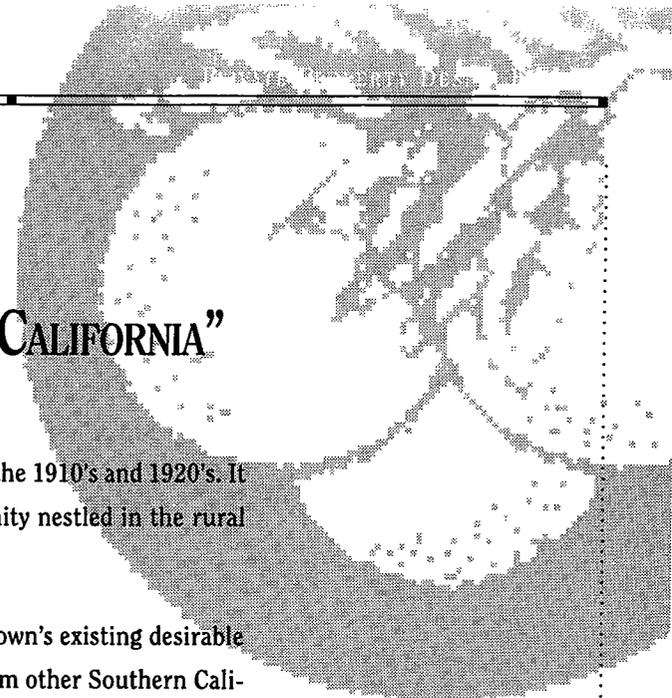
“THE LAST BEST SMALL TOWN IN SOUTHERN CALIFORNIA”

Downtown Fillmore is a classic American small town center, as flourished in the 1910's and 1920's. It is a place where both residents and visitors experience a relaxed historic community nestled in the rural Santa Clara Valley.

This Downtown Specific Plan is designed to retain and strengthen the Downtown's existing desirable qualities. Fillmore has an intimate, small town character that is clearly distinct from other Southern California cities. Fillmore's railroad and citrus industry heritage, the traditional Central Park, its pedestrian-oriented shopping area and historic buildings provide a strong and positive character that is to be protected and enhanced.

This Specific Plan, in combination with a City and downtown business sponsored marketing plan, will establish a foundation upon which to attract tourists and residents to seek amusement, services and specialty shopping within the entire Central Business District.

The marketing plan would schedule, coordinate and sponsor thematic street fairs, open air markets and other civic events with an emphasis on Fillmore's cultural heritage as well as the historic and future importance of the railroad. These events would enable the entire Central Business District to come alive with tourist and resident activity both day and night. This Specific Plan also provides for additional housing opportunities in the downtown. The resultant lively ambiance would nourish the existing small businesses while creating opportunities from new economic development in such areas as lodging, specialty retailing, and the film industry. This plan will ensure that Fillmore remains the last best small town in Southern California.





CITY OF FILLMORE DOWNTOWN SPECIFIC PLAN

PROJECT CREDITS

CITY COUNCIL/REDEVELOPMENT AGENCY

Linda Brewster, Mayor
Roger Campbell, Mayor Pro-Tem
Michael McMahan, Councilmember
Scott Lee, Councilmember
Don Gunderson, Councilmember

PROJECT STEERING COMMITTEE

Richard Gonzales, Chairperson
Michael McMahan
Roger Campbell

Patricia Smith
Shirley Spittler
Dylan Snow
Larry Dunst
Jim Clark
Sylvia Schuster
Dave Anderson
Dorothy Haase
Delores Day
Margaret Yeagen
Ray Wolfe, In Memorium

FILLMORE CITY/REDEVELOPMENT AGENCY STAFF

Roy Payne, City Manager
Tony Perez, Senior Planner
Kevin McSweeney, Assistant Planner

NOTE OF THANKS:

The City of Fillmore and the Project Team wish to extend their appreciation to the many citizens of Fillmore who gave of their time and energy to participate in the Public Workshops which formed the basis for the development of this Specific Plan.

PLANNING COMMISSION

Ernest Villegas, Chairperson
Randy Lemons, Commissioner
Patricia Smith, Commissioner
Frank Wahl, Commissioner
Ed Jaloma, Commissioner

PROJECT TEAM:

MAINSTREET ARCHITECTS AND PLANNERS

Nicholas Deitch, Principal, Planning and Urban Design
Curtis Cormane, Design and Graphics
Dao Doan, Design/Public Workshops
Deborah Guthrie, Design/Public Workshops

STEPHANIE LAWSON CONSULTING

Stephanie Lawson Diaz, Land Planning and Documentation, Public Workshops

EPT LANDSCAPE ARCHITECTURE

Jeff Chamlee, Principal
Andy DeJong, Design and Graphics

The Planning Corporation

Steve Craig, Environmental Impact Analysis and Conservation Element

HISTORIC PHOTO SOURCES:

The Fillmore Historical Society
The Collection of Philip Serpico
The Ventura County Museum of History and Art

WITH ASSISTANCE FROM:

T & B Planning Consultants
Frank Greco, Principal, Public Workshops
Firouz Tabousi, Design

Economic Research Associates

David Wilcox, Economic and Market Analysis

JB Research

Jill Bentley, Marketing and Tourism Study

ASL Consulting Engineers

Engineering background data

LSA Associates, Inc.

Environmental background data

Duane Eells Design

Graphic Design

On January 17, 1994, the very week that this Plan was to go to hearing, Southern California was rocked by a major earthquake, centered in Northridge. The City of Fillmore sustained substantial damage, affecting some 400 residences and many businesses, including the loss of several key buildings within the Specific Plan boundary. Although the Plan document was essentially complete, we have tried to revisit the text and exhibits to address some of the significant losses, such as the Masonic Temple which stood at the corner of Central and Sespe Avenues. At the time of publication, many of the physical impacts on downtown are yet to be determined. It is recommended that conditions within the Plan Boundary be monitored over time and that this document be modified to correct discrepancies which may arise as the impacts of the earthquake continue to be assessed.

Adopted 3/17/94 RDA Resolution 93-75, 93-76



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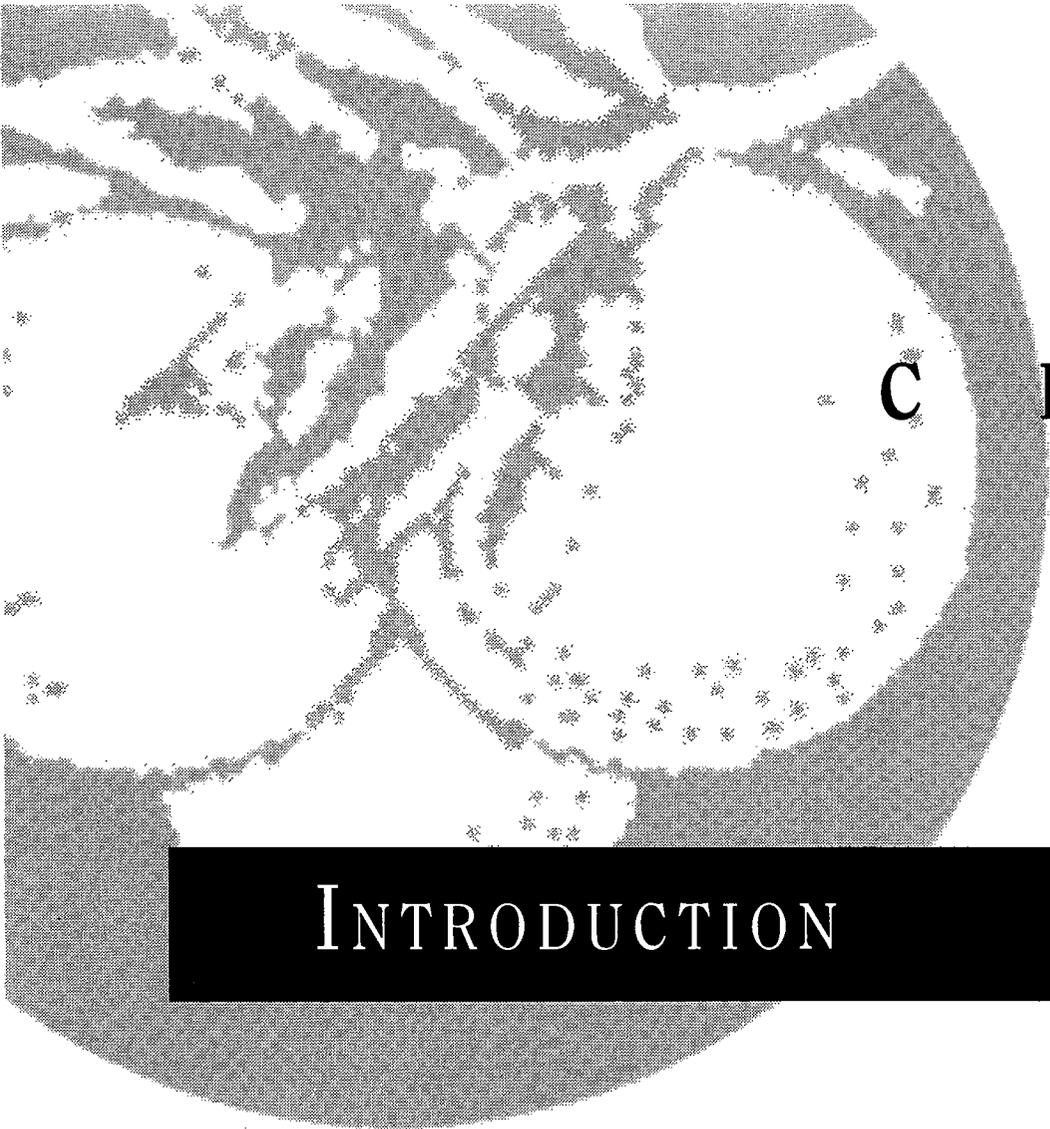
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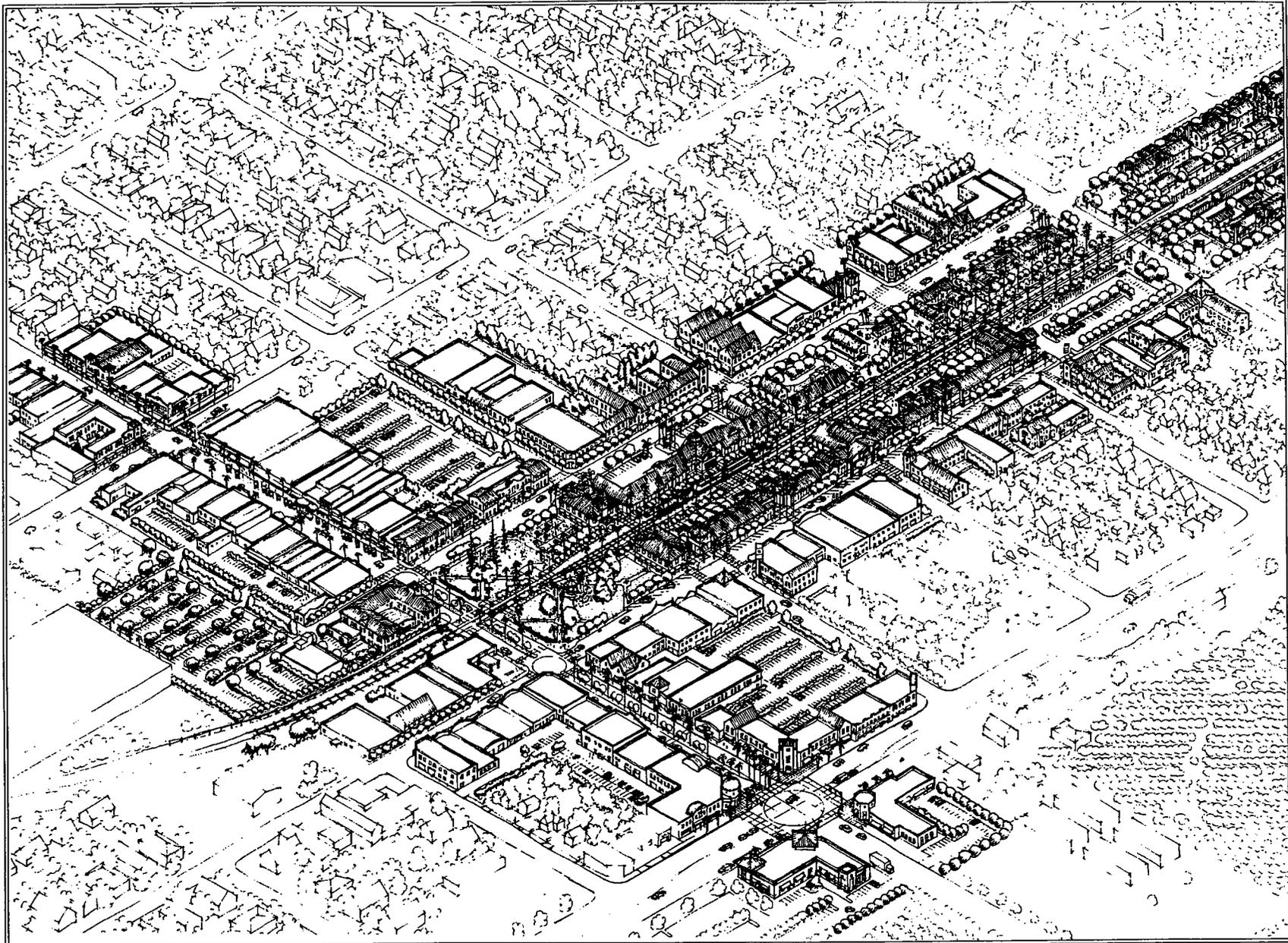
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C H A P T E R

1

INTRODUCTION



COMPLETED VISION OF DOWNTOWN



A

PLAN OVERVIEW

SPECIFIC PLAN GOALS:

The following goals have been developed by the community as the foundation for the Downtown Specific Plan. This was done through a Project Steering Committee and Town Meetings. These goals provide the basis for the strategies and development standards contained in this Plan.

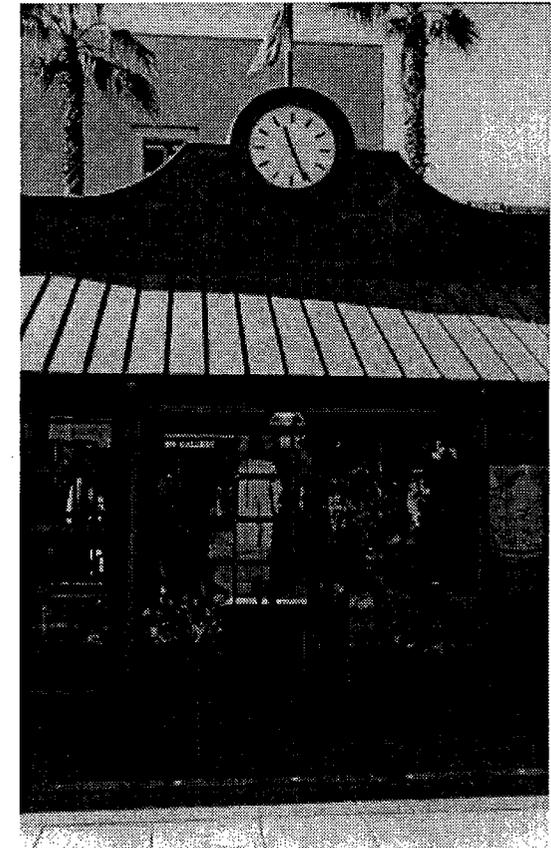
- Protect and enhance the existing small town character of the Central Business District.
- Encourage revenue-generating development that will attract residents and visitors alike.
- Guide new development and renovation to assure a relatedness between individual buildings both new and old.
- Identify opportunities and constraints which will affect the realization of the Plan.
- Provide a yardstick against which future renovations and new development proposals can be measured.

- Provide the opportunity for City and private partnership investments and other development incentive programs.
- Provide housing opportunities in the downtown.

THE COMPLETED VISION:

- The Central Business District is the recognized town center which will be enhanced through implementation of this Plan. The completed vision for the Central Business District includes the following components:
- A condensed, active business center composed of a balanced mix of specialty retail establishments, community-serving businesses and civic uses.
- A lively pedestrian shopping district at the street level.
- An expanded and enhanced Central Park to become the focus of downtown, creating a true town square with open plaza space, park greens, shade trees and historic elements. Development is to be intensified on the properties surrounding the park.
- A continuous and vital retail experience from the Highway northward along Central Avenue to Sespe Avenue and along the adjoining portions of Main Street and Santa Clara Avenue.

- Intensified development at the Central Avenue and Highway 126 intersection, with monumentation to establish a prominent gateway into the downtown.
- A marketing plan to coordinate street fairs, open-air markets and civic events to bring the Downtown alive for visitors and residents both day and night.

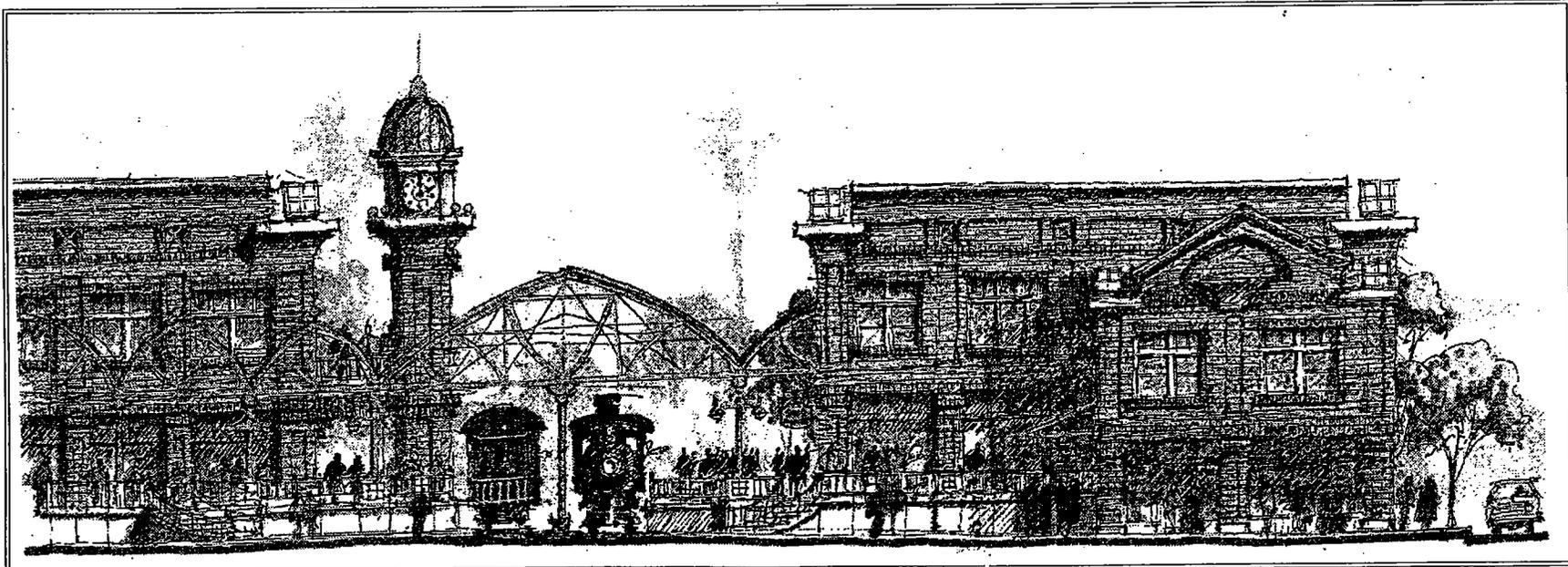




A VISION FOR THE RAILROAD PROPERTY:

The Redevelopment Agency owns a thirteen acre parcel of land which encompasses the railroad tracks extending into the center of Downtown. This parcel is an important component of the plan for "completing" the Downtown. The Plan calls for comprehensive development of this property with the following components:

- A tourist destination emphasizing the historic and future importance of the railroad.
- A grand "rail station", restaurant and saloon complex with exhibits, ticket booth and an interpretive center which provides an introduction to the Sespe Wilderness.
- A prominent "promenade" along each side of the train tracks which establishes a celebrated gateway and a link from Central Avenue east through the new development to Mountain View Street.
- Retail specialty shops on both sides of the train tracks, along the promenade.
- A dinner train attraction.
- Historic trains on exhibition.
- An historic complex featuring the original Fillmore train depot, a new museum focused on a garden court and other historic buildings.
- A hotel or inn to encourage overnight trips to Fillmore.
- A community commons for a farmers market and festivals.
- A performing arts theater.
- Housing opportunities for seniors and/or first-time home buyers.



CONCEPT SKETCH: "RAILROAD PROMENADE"

FIGURE 1.2



CONCEPT SKETCH: RAILROAD PROMENADE

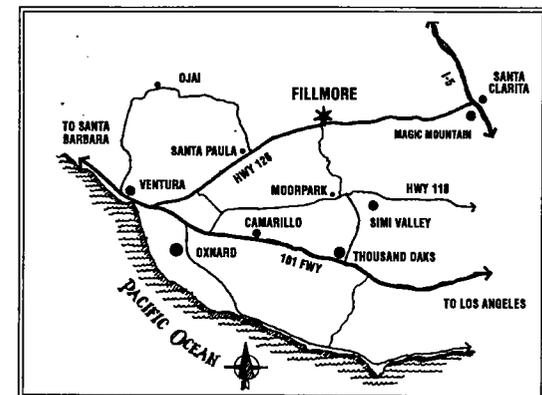
FIGURE 1.3

FILLMORE'S REGIONAL SETTING

Fillmore is located in the temperate Santa Clara Valley, halfway between the cities of Santa Clarita and Ventura, on State Hwy 126. Fillmore is approximately 70 miles north of Los Angeles, and within an hour's drive of the densely populated San Fernando Valley (Figure 1.4).

AREAS SURROUNDING THE DOWNTOWN:

The areas immediately adjacent to the Central Business District are not a part of the Specific Plan. However, the Environmental Impact Report for this Plan provides a general analysis of these areas for future amendments to the General Plan or Specific Plan. These areas are envisioned to accommodate a variety of developments which support, but do not compete with, the downtown including mixed use, residential, office, industrial and highway commercial uses.



FILLMORE'S REGIONAL LOCATION

FIGURE 1.4



B

THE PURPOSE OF THIS DOCUMENT

LEGAL AUTHORITY

Section 65450 of the California Government Code grants Fillmore the authority to prepare a specific plan for any area covered by the General Plan to establish systematic methods for implementation of the General Plan.

The primary effect of a specific plan is the establishment of a clear and detailed plan for a specific area. Conventional zoning is replaced with detailed development requirements of the specific plan which best meet the needs of the specific area within the plan's boundaries. As a result, zoning, subdivisions, public works projects, development projects and development agreements must be consistent with the adopted specific plan.

State law requires that a specific plan be in conformance with the General Plan. This Plan has been prepared in conformance with the City of

Fillmore General Plan, Redevelopment Plan and CBD zone district, to provide a detailed program for the enhancement and completion of the downtown core.

GOALS, DEVELOPMENT POLICIES AND GUIDELINES

The Downtown Specific Plan is a comprehensive policy and regulatory document which provides overall land use and development goals as well as specific development policies and design guidelines for the Central Business District and the thirteen acre Railroad Property.

In this document, Goals are statements which define the overall purpose and objectives of the Plan. Development Polices are mandatory criteria which must be met and Guidelines are advisory standards used as a basis for measuring or comparison in order to provide direction for new or renovation projects.

This Plan provides information and concept plans to remind decision-makers, City Staff and the community of the completed vision. This Plan clarifies the City's expectations and provides property owners and developers with guidelines for future development along with incentives for renovation and new development. This plan is dedicated to enhancing the Downtown through renovations and by attracting new development that will be built to specified standards.

C

THE SPECIFIC PLAN AREA

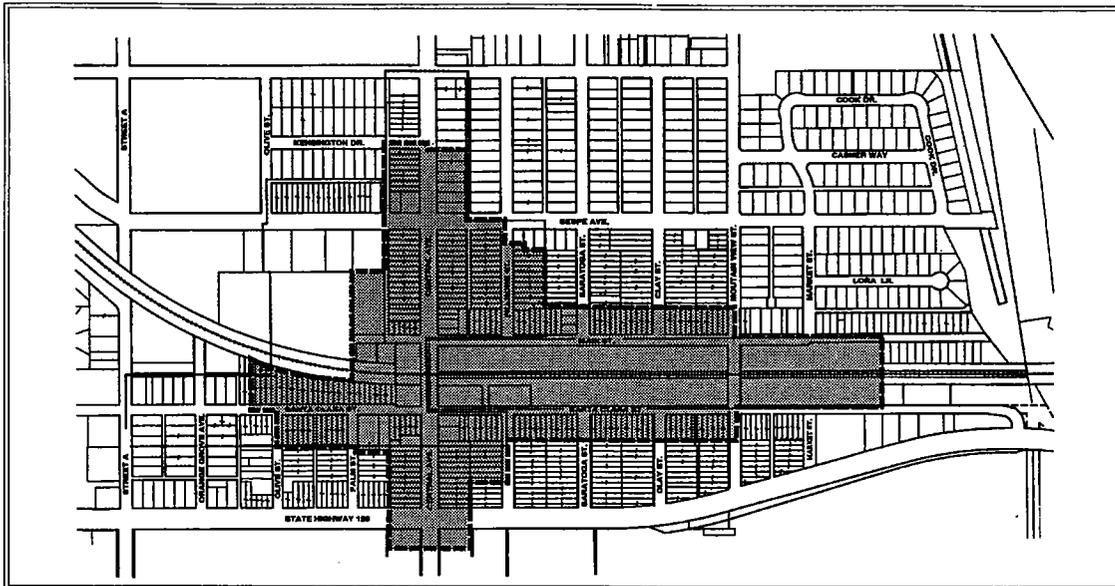
LOCATION AND BOUNDARIES

Just as the City is centrally located along Highway 126 between Ventura to the west and Santa Clarita to the east, the Downtown Specific Plan is centrally located within the City itself. The Specific Plan boundaries are illustrated in Figure 1.5. A legal description of the Plan boundaries is on file with the City.

The two areas encompassed within the Plan, as illustrated in Figure 1.6, are 1) the Central Business District, which has been reconfigured by this Plan, and 2) a portion of the Central Business District referred to as the "Railroad Property". This thirteen acre parcel of land was purchased by the Redevelopment Agency from Southern Pacific Transportation in 1990 and is bisected by a 30-foot-wide railroad track right-of-way.

SPECIFIC PLAN SETTING

The majority of the Specific Plan area follows the reconfigured Central Business District boundary. This area generally extends from the Highway

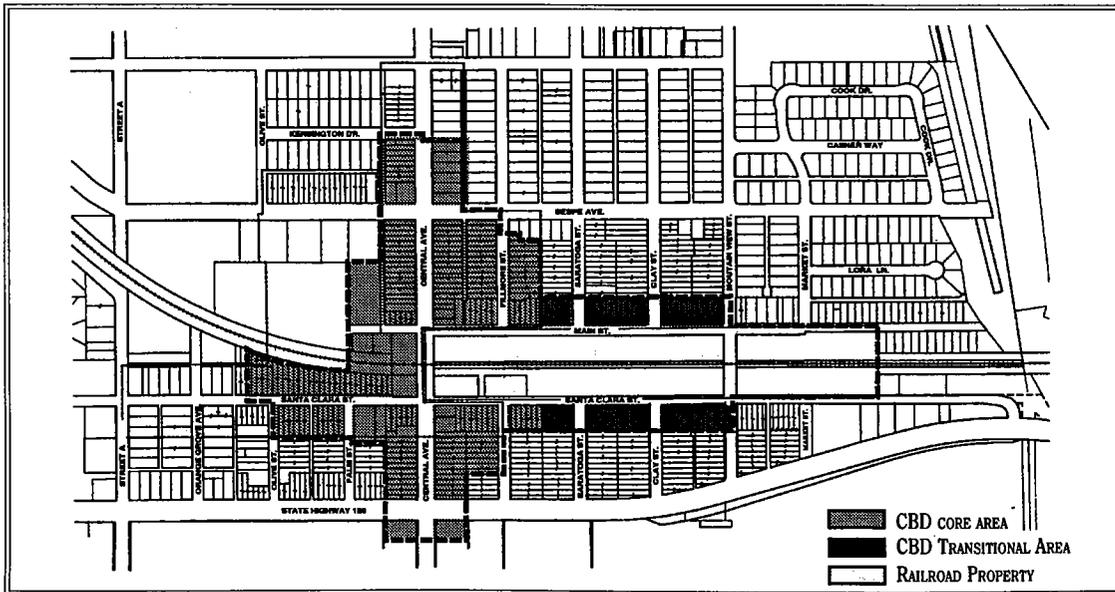


LOCATION AND BOUNDARIES OF THE SPECIFIC PLAN

FIGURE 1.5

126/Central Avenue intersection on the south to Kensington Drive on the north and generally includes from one to three blocks to the east and west of Central Avenue including the Railroad Property up to Mountain View Street.

The Central Business District consists of the majority of existing downtown businesses, some vacant lots and various residential pockets. Within the CBD is the 13 acre Railroad Property which is primarily vacant with the exception of a limited number of short-term leases to various businesses and a longer-term lease to Shortline Enterprises. Shortline provides trains and related equipment to the movie industry, and promotes special railroad-related events such as the dinner train, rides and tours.



THE MAJOR COMPONENTS OF THE SPECIFIC PLAN AREA

FIGURE 1.6

The area outside the Railroad Property and Central Business District consists of active industrial uses, highway-oriented and neighborhood serving commercial uses, city government facilities and numerous types of housing. This housing varies from stable to transitioning neighborhoods.

The area located generally north of the Plan is composed primarily of stable residential communities, the City library, the high school and other school-related facilities. A notable area just north-west of the CBD is the Sespe Avenue commercial office district. This area provides commercial support to the CBD. There are mainly community service functions such as City Hall, doctor's offices and banks with on-site parking



facilities. This area serves as a transition from the more intense CBD activities to the less intense residential land uses located to the north and west.

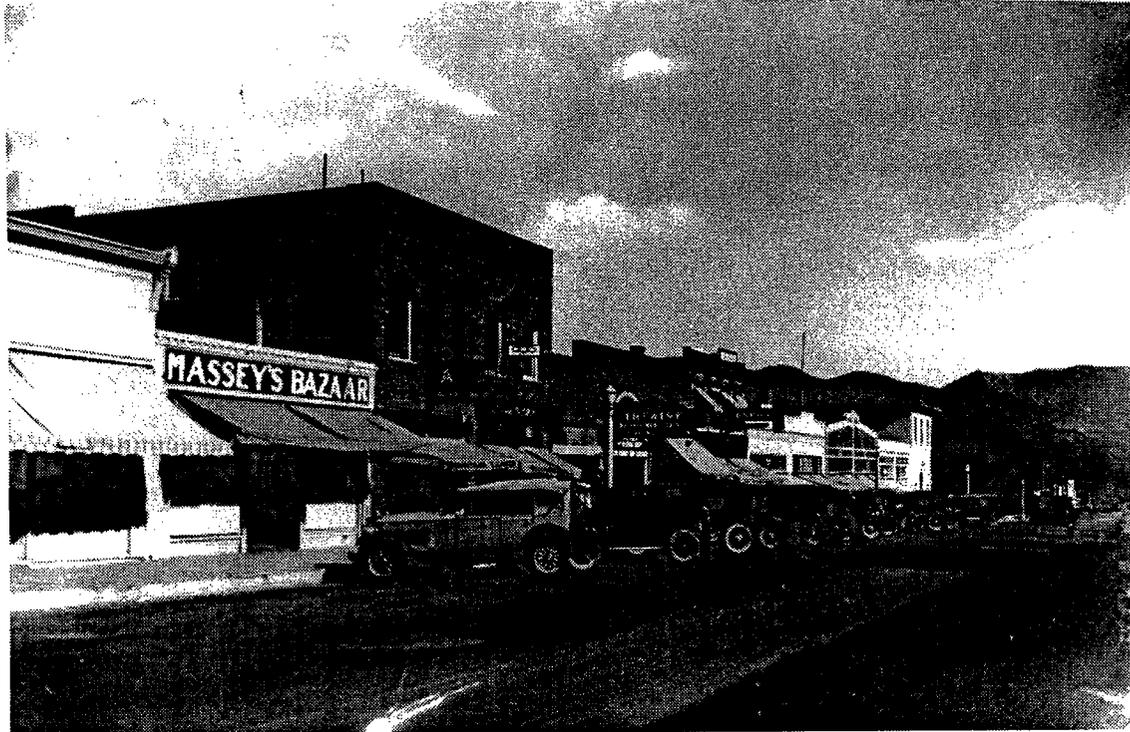
The area to the west transitions from traditional and older style residential development to more contemporary housing, and land uses to the east are primarily agricultural in nature. About 15 acres of orchards and underutilized or vacant land southwest of downtown have been reserved for a proposed community shopping center, and the orchards southeast of this core area are planned to be developed as additional residential neighborhoods.

D

ENVIRONMENTAL COMPLIANCE

An Environmental Impact Report was prepared for the Downtown Specific Plan which assessed the potential for environmental impacts associated with the build-out of the Central Business District and the Railroad Property. This document is on file with the community development department

This document is available under separate cover from the City Community Development Department.



CENTRAL AVENUE LOOKING SOUTH, ABOUT 1920.



FILLMORE'S VOLUNTEER FIRE DEPARTMENT. c.1920



E

CONSISTENCY WITH PLANS AND POLICIES

The goals, policies and guidelines of this Plan are tools for implementing the Fillmore General Plan and Redevelopment Plan by tailoring the policies of these plans for this particular area. Specifically, this Plan is in conformance with the City's established objectives as follows:

GENERAL PLAN/ZONING ORDINANCE

The City's General Plan calls for the Central Business District (CBD) to cater to commercial uses which are not dependent upon high traffic volumes and visibility, as compared to Highway 126. It recommends that the CBD continue to be the center for specialty retail, service businesses and offices and not compete with the uses along the Highway corridor. The General Plan recognizes the importance of the CBD as the community center and acknowledges its specialized role in the commercial life of the City. The Plan further recommends rejuvenation through visual and structural improvements.

This Downtown Specific Plan carries out the vision in the General Plan by refining the "Central Business District" boundary, providing development standards and design guidelines for the renovation of existing buildings and the construction of new developments and providing a plan for public improvements including streetscapes, Central Park enlargement and entry monuments.

The plan for the development of the Railroad Property with a quality tourist-oriented development will also contribute to the "rejuvenation" of the CBD as called for in the General Plan.

The City's Zoning Ordinance contains a "CBD" zone district which is established to provide for community and tourist commercial needs. The zone is principally intended to be used as Fillmore's business center where a wide range of specialty retail establishments as well as service businesses that serve the residents as well as visitors to the area. The development standards associated with this zone establish appropriate street level uses and building design to ensure a pedestrian-oriented, lively and diverse commercial district. This Plan carries out the intent of the CBD zone district through the many goals, policies and guidelines which direct the location, type and extent of both private and public development projects.

REDEVELOPMENT PLAN

Fillmore's Redevelopment Area encompasses all of the Specific Plan area. The Redevelopment Plan authorizes the Redevelopment Agency to acquire property, construct public improvements, develop property and rehabilitate existing buildings. Also, a portion of all taxes received are to be used for increasing and improving housing for persons of very low, low or moderate income. The major goals of the City's Redevelopment Plan which are carried out in this Specific Plan are as follows:

- Eliminate blighting influences and improve the overall appearance of public and private facilities.
- Provide adequate streets, drainage and pedestrian or vehicular circulation.
- Strengthen commercial functions.
- Provide adequate land for parking and open spaces.
- Strengthen civic, community and cultural functions.
- Preserve architecturally and historically valuable structures.
- Establish and implement performance criteria to assure high quality design and environmental standards.



- The Redevelopment Plan also contains mandatory criteria that new projects must meet:
- Maintain the dignity and sovereignty of existing property owners in the City.
- Maintain the character of the City.
- Enhance historic context of the Community.
- Enhance safety and health of the town.
- Enhance the attractiveness of City buildings and residences.
- Provide adequate housing.
- Enhance opportunities for open space.

The plan for the CBD, as well as the Railroad Property land uses and development concept, encompasses all of the above criteria.

GROWTH MANAGEMENT ORDINANCE

The City's Growth Management Ordinance controls the number of residential units that can be built each year until the year 1999. The amount and type of units are restricted to a rate that currently allow for a total population increase of 198 persons per year, or approximately 65 units per year. This ordinance is being revised to be consistent with the General Plan. Additionally, a Growth Management Element is in preparation to facilitate

implementation of the Growth Management Ordinance. The construction of housing within the Specific Plan area is subject to the requirements of this Ordinance.

A more detailed explanation of consistency with City plans and policies is contained in the Downtown Specific Plan EIR.



CENTRAL AVENUE LOOKING SOUTH, 1993.



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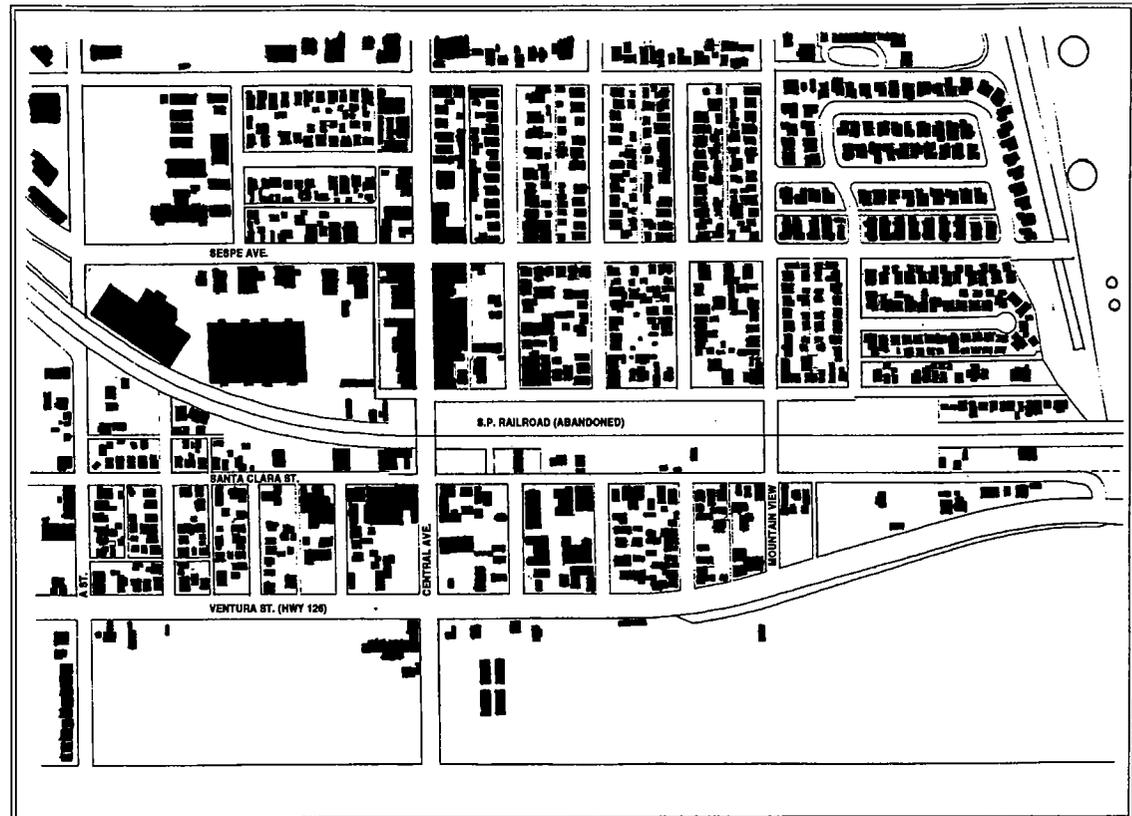
C H A P T E R

OPPORTUNITIES AND CONSTRAINTS



Prior to developing the Downtown Specific Plan, an analysis, entitled "Opportunities & Constraints Summary Report", was compiled in December 1992. (This report is incorporated by reference and is on file with the Fillmore Community Development Department.) The summary report focused on the Central Business District and the Railroad Property and examined the following issues:

- Existing land uses.
- Zoning and general plan designations.
- Current ownership patterns.
- Circulation and roadway capacity.
- Environmental conditions.
- Utility availability.
- Two special use programs: operation of an amusement/dinner train and establishment of an historic museum complex.
- Existing and anticipated market and economic conditions.



EXISTING BUILDING PATTERN

FIGURE 2.1

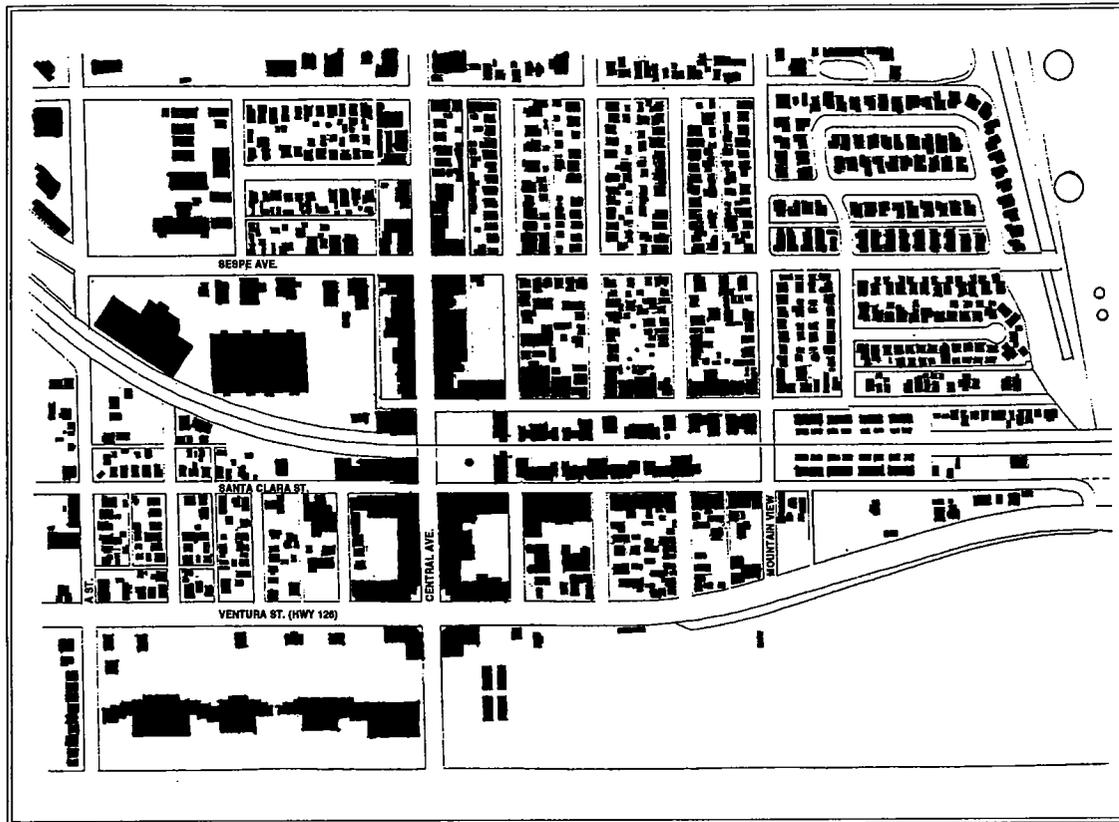
LAND USE AND ENVIRONMENTAL CONDITIONS

The analysis uncovered few restraining factors relative to the planned growth of the Downtown Specific Plan Area. Specifically, the report found the following information relative to the Downtown and the Railroad Property:

Downtown - There is not a strongly consistent pattern of related land uses or structure types

within the CBD along Central Avenue and surrounding the Railroad Property. This area is interspersed with various commercial, residential and industrial uses. As illustrated in Figure 2.1, many lots in the areas off Central Avenue are underdeveloped or vacant. Also, many structures in this area are in need of renovation. Figure 2.3 illustrates a land use analysis of existing conditions.

A noticeable gap in development occurs on Central Avenue at the railroad right-of-way. The



POTENTIAL BUILDING PATTERN

FIGURE 2.2

highest concentration of commercial structures and uses occurs on Central Avenue from Main Street to Sespe Avenue. The opportunity presented in this Plan is to complete the building pattern in the CBD by closing the gap on Central Avenue at the railroad tracks and encouraging in-fill and renovations on surrounding properties and extending south on central to Ventura Street. The potential "completed" building pattern is illustrated in Figure 2.2.

The Northridge Earthquake has resulted in substantial loss of buildings within the downtown core. There are several lots, many of which are contiguous, which represent an opportunity to develop significant new buildings within the existing historic context. The guidelines in Chapter 4 illustrate the appropriate way to develop buildings which respect the historic building pattern even though they are on parcels which may be substantially larger than the historic norm.

A major factor affecting implementation of this Plan outside of the Railroad Property is the lack of condemnation authority within the Redevelopment Plan. Lacking the power of eminent domain, the City would need to purchase key parcels or work in partnership with private land owners by providing financial or other incentives to modify land uses or renovate structures outside the Railroad Property.

A primary focus for this kind of City involvement will be along Central Avenue between Santa Clara Avenue and Highway 126 and on the south side of the Railroad Property. Existing, relatively large ownership patterns could prove beneficial in facilitating the implementation of Plan goals in this area.

The Downtown has few existing environmental constraints with the need for only minor drainage and circulation improvements.

Railroad Property - The Railroad Property is relatively unconstrained. The site is primarily vacant, with existing uses operating under short-term City leases. A few, minor drainage and circulation improvements need to be undertaken. Although the division of the relatively narrow site by railroad tracks could prove to be a limiting factor in some development scenarios, the Plan employs the rail line as an opportunity by creating land uses and a pedestrian promenade that will work synergistically with the proposed dinner train, railroad station, and potentially, a commuter line.



ECONOMIC/MARKETING CONDITIONS

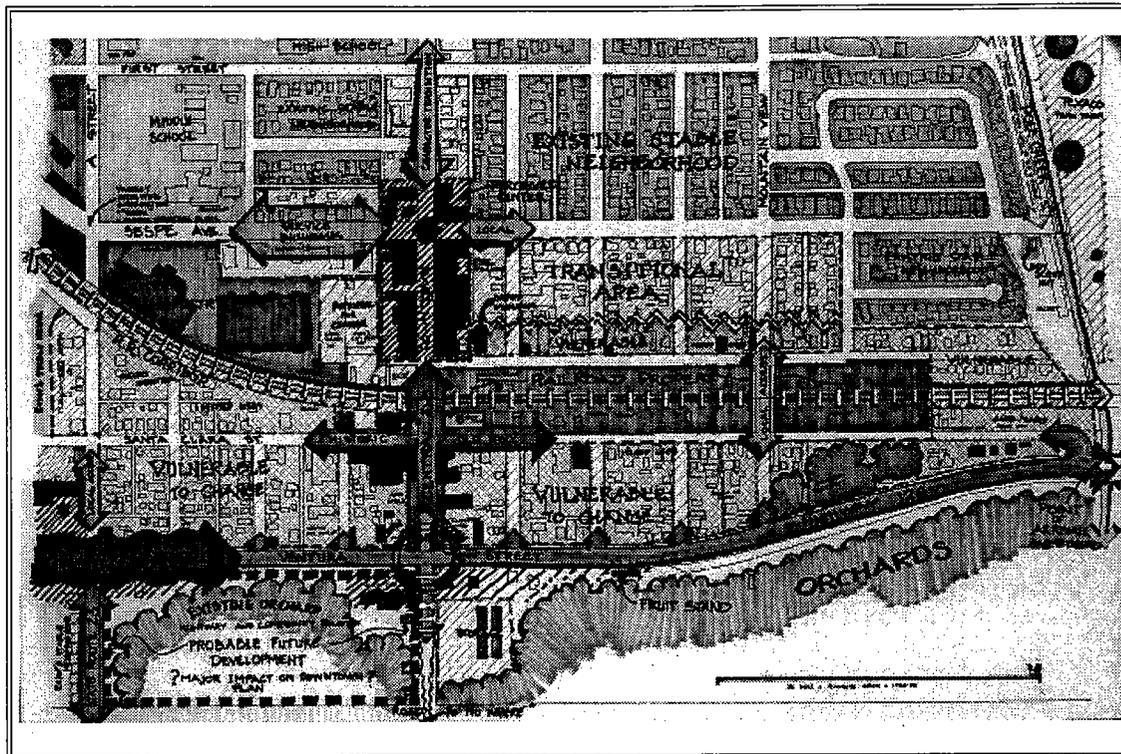
As a part of the Specific Plan process, the City retained Economic Research Associates (ERA) and JB Research Company to prepare a marketing report and an evaluation of tourist attraction potentials.

These reports suggest that Fillmore will grow in size from the current 13,000 to 18,000 in the year 2010. If these estimates are correct, the City will experience increased demand for commercial retail and office space both in the Downtown and along Highway 126.

ERA indicates that although Fillmore currently has a range of 365,000 to 425,000 square feet of existing retail, office and commercial service space, a demand for some 274,500 square feet of new commercial space will occur before the year 2000. After the year 2000, it is possible that an additional 311,000 square feet of commercial space will be needed due to residential population growth.

ERA's principle recommendations for Fillmore are as follows:

Downtown is already of sufficient size and character that it should be retained at about the scale which presently exists.



EXISTING CONDITIONS ANALYSIS

FIGURE 2.3

Downtown should not directly compete with the Highway 126 corridor for the attraction of chain stores and community shopping center uses.

Downtown should concentrate upon the collection and continuing tenancies by a mix of walk-in retail, food and beverage, professional office, and many uses which relate to community communication and to personal services.

Downtown should seek the continuation of healthy, small- and medium-scale businesses

which work together and are complementary to each other.

The majority of in-fill development opportunities are on Santa Clara Avenue west of Central Avenue.

Regarding the Railroad Property, ERA agrees with the community objective to incrementally develop a commercial recreation, regional attraction focused upon the authentic experience of a railroad community. The incentives that the City



has provided to Shortline Enterprises are appropriate. Shortline can operate an effective commercial recreation entertainment attraction that is consistent with the historic use of the Railroad Property. This operation will support the City's economic development objectives by drawing in regional visitors who seek unique experiences. The thematic use of the Railroad Property is complementary to the goal of enhancing the downtown area.

ERA does point out constraints relative to the Railroad Property, including the constricted depth of lots due to the 30-foot railroad right-of-way. Also, to promote residential use on the eastern end of the Railroad Property the City may need to initiate renovation (as well as in-fill with new development) of the properties which lie along Main Street and Santa Clara Street in the same vicinity.

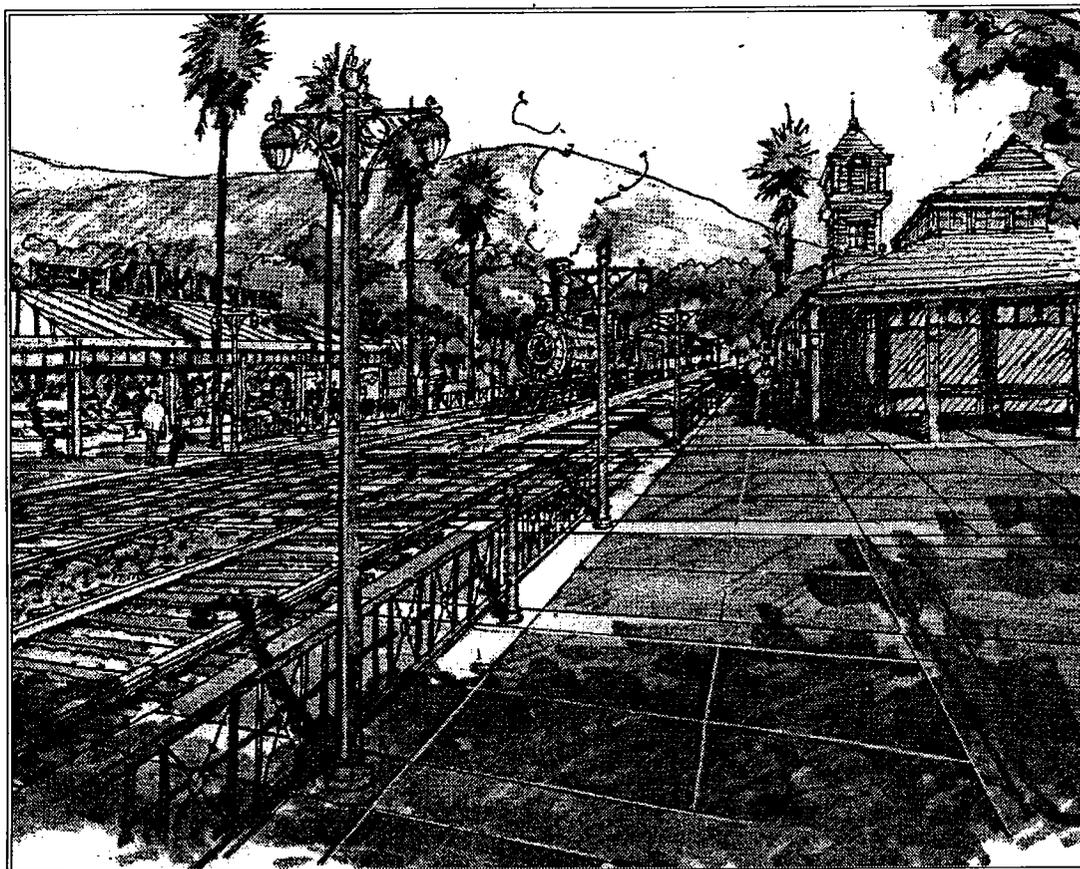
According to JB Research, Fillmore should pursue development of a dinner train, depot and museum attraction; a festival grounds and an interpretive center for the Los Padres National Forest. Based upon the research conducted, supportable attraction development in Fillmore could include the following:

Interpretive Center: Los Padres National Forest Interpretive Center to be located on the Railroad Property or near Sespe Creek, where access could be provided directly from Highway 126, the proposed amusement train and by horse and bike trails. As an alternative not addressed in the evaluation, a smaller interim center could be constructed

on the Railroad Property then moved permanently to the Sespe Creek site.

The center's program would include state-of-the-art exhibits, interactive displays, nature trails and interpretive walks and programs highlighting the condor sanctuary, the Sespe River, wildlife, fish and game, etc.

Festival Grounds: A three- to four-acre festival ground offering six to eight major events annually, such as; 4th of July-CHili Cookoff & Roadster Show, a wine festival, Cinco de Mayo Festival, Oktoberfest, a flower festival or a Taste of Ventura County restaurant festival, could be developed on the Railroad Property, at the school property or in a combination of locations. A festival area could be staged on an interim or permanent basis on the





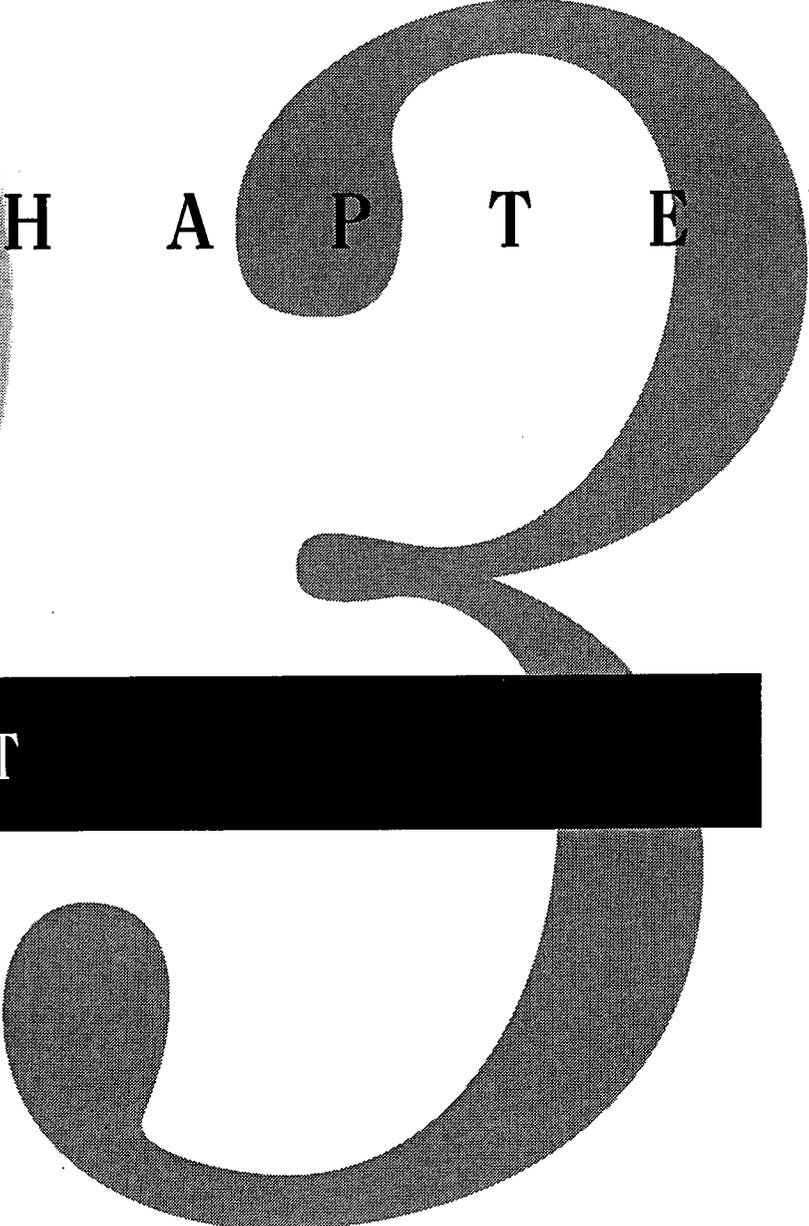
Railroad Property. The need to provide off-site parking and the potential interference caused by the rail tracks and train operation need to be addressed.

Dinner Train/Depot/Museum: An 8,000-square-foot depot/museum constructed in association with the Short Line Enterprises, Ltd. movie/dinner train could be located on the railroad site.

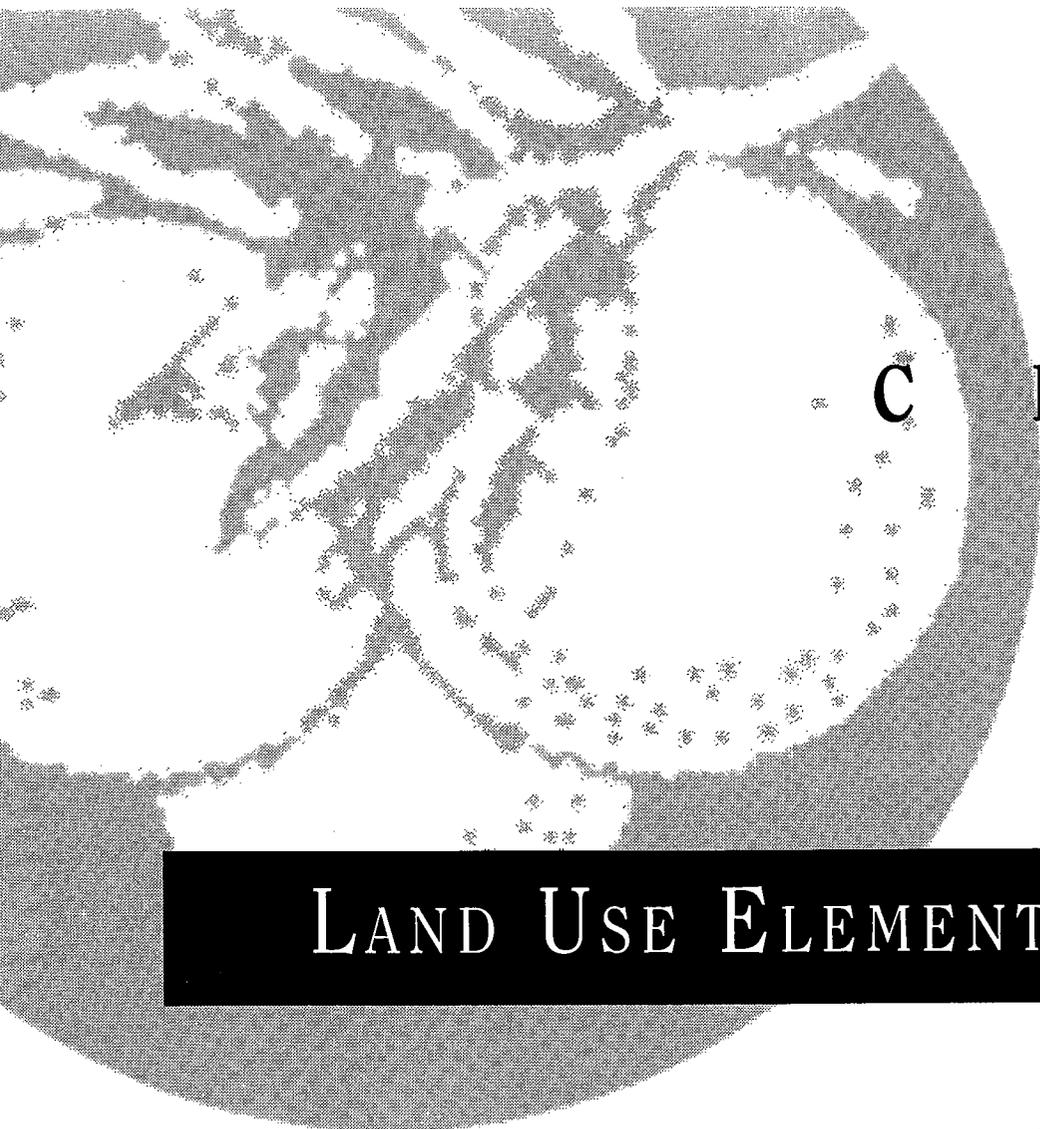
In response to this information, the Downtown Specific Plan provides for build-out of approximately 145,000 to 245,000 square feet of additional new commercial development in the Central Business District including a railroad theme-oriented commercial development on the Railroad Property.



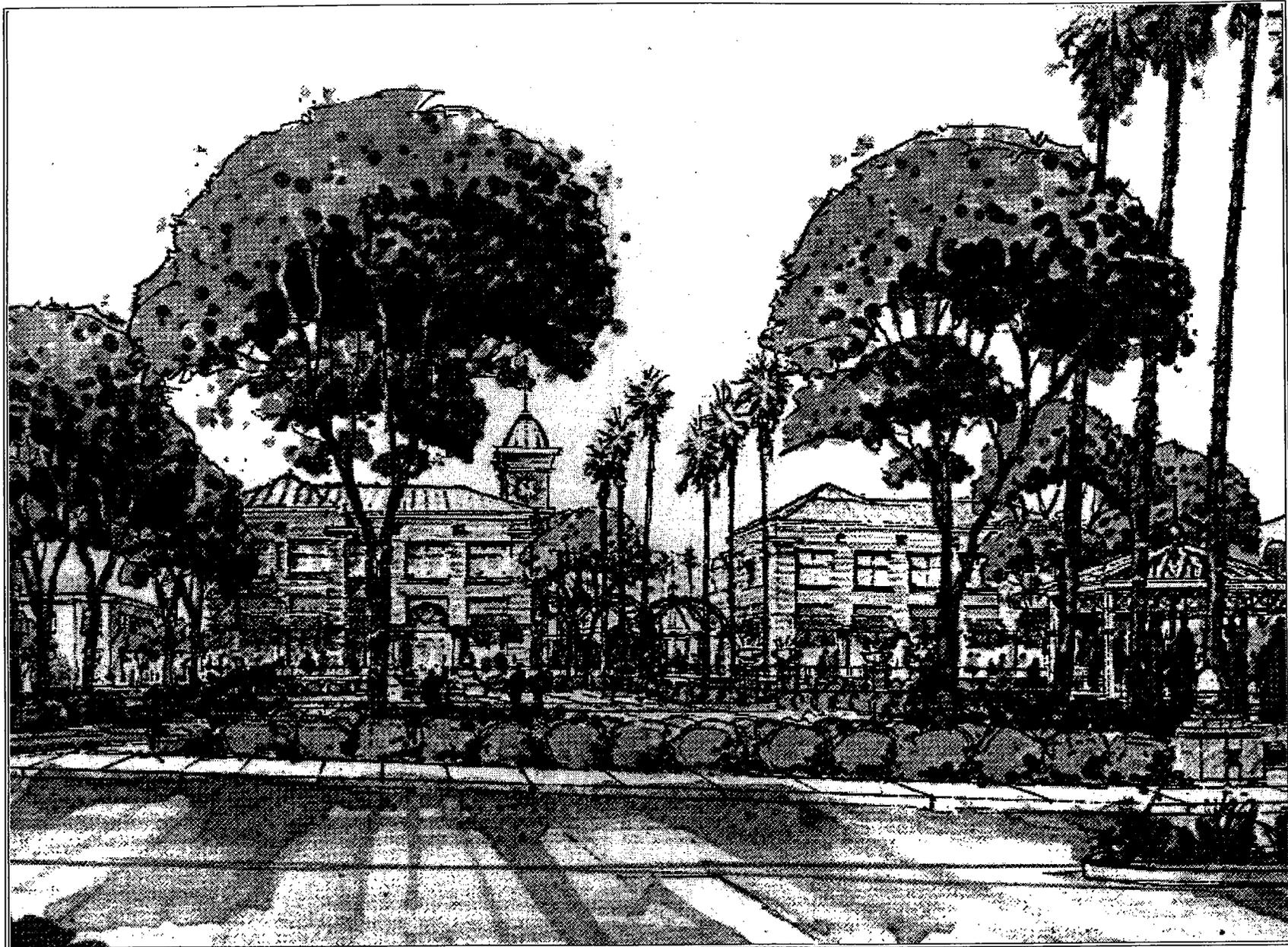
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CHAPTER



LAND USE ELEMENT



CENTRAL PARK WILL BECOME THE TRUE CENTER OF THE CBD.

FIGURE 3.1

This element of the Downtown Specific Plan provides a detailed “road map” which defines the land uses, zoning, and development policies for build-out of the Central Business District and the Railroad Property in order to ensure orderly development of both public and private projects.

This element contains two major sections. The first section is devoted to the Central Business District and the second section focuses on the Railroad Property.



CENTRAL BUSINESS DISTRICT (CBD)

LOCATION AND BOUNDARIES

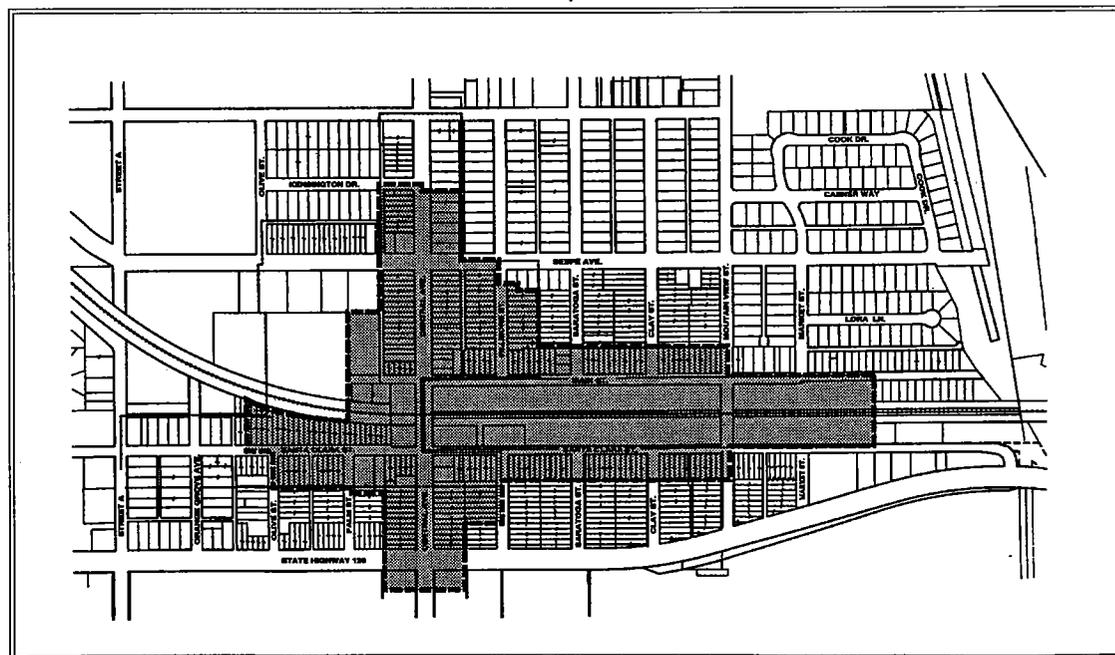
The Central Business District boundary is condensed and reconfigured by this Plan. The new CBD Boundary generally extends from the Highway 126/ Central Avenue intersection on the south to Kensington Drive on the north and generally includes from one to two blocks to the east and west of Central Avenue including the Railroad Property which runs east-west from Central Park to a point approximately 650 feet east of Mountain View Street.

CBD LAND USE GOALS

This district is established to provide for community and tourist commercial needs. The district is intended to be used as a business center where a wide range of specialty retail establishments, professional offices, community services and community institutions are located to serve the residents of the community as well as visitors to the area. In conjunction with appropriate commercial uses, residential uses are allowed as a secondary use. A priority of this district is the establishment and support of street level commercial uses that will ensure a pedestrian-oriented, lively and diverse commercial district.

This Plan redefines the CBD by condensing its boundaries and establishing policies that will support and enhance the existing character of the downtown core in conjunction with the central Business District Zoning Ordinance. This redefined CBD will continue to serve as the central location for specialty retail, commercial office, restaurant, theater, night clubs and public and governmental service uses within Fillmore. Specific goals for the CBD are as follows:

- Maintain, promote and enhance the small town character of downtown Fillmore as a draw for both residents and tourists.



LOCATION AND BOUNDARIES

FIGURE 3.1



- Condense the CBD to form a distinct area along Central Avenue from Highway 126 to Kensington Avenue in order to maximize the opportunity for CBD uses without creating too large of a commercial base.
- Establish Central Park and the immediately surrounding area as the “center” of the CBD. Figures 3.2 and 3.3 illustrate this concept.
- Provide a mix of specialty retail establishments, community-serving businesses and civic uses to serve both residents and visitors.
- Provide a lively pedestrian shopping district at the street level.
- Intensify development on the properties surrounding Central Park.
- Create a continuous and vital retail experience from the Highway northward along Central Avenue to Kensington Avenue and along the adjoining portions of Main Street and Santa Clara Avenue.
- Provide for intensified development at the Central Avenue and Highway 126 intersection which is an extension of the CBD, including uses such as visitor serving commercial, hotels, restaurants, and specialty retail.
- Encourage special events such as street fairs, open-air markets and civic events to bring the downtown alive for visitors and residents both day and night.
- Retain important historic structures such as the Fillmore State Bank, Masonic Temple and Farmers and Merchants Bank, which are listed as Ventura County Landmarks. Ensure that the buildings are refurbished, maintained and occupied with viable, downtown uses.
- Relocate the original Fillmore Depot, which is listed as a Ventura County Landmark to the Railroad Property, if feasible, and continue its operation as an historic museum.
- Provide for a civic facility within the CBD to house relocated City governmental services such as administrative services, police and fire, planning, health and safety.
- Encourage the relocation of public services, such as the public library, school district offices or the post office to the area immediately adjacent to Central Park, creating a public service/civic complex surrounding Central Park.
- Promote a streetscape program for the entire CBD.
- Establish a prominent gateway into the downtown through intensified development and establishment of an entry monument to be constructed at the intersection of Central Avenue and Highway 126, which will announce and celebrate the entrance to the CBD.
- Rehabilitate and refurbish where feasible, existing commercial and residential structures through City-supported incentive programs for facade and landscape improvements.
- Retain the existing unreinforced masonry buildings on Central Avenue, wherever feasible and prioritize monies for their rehabilitation.
- Promote mixed land use in two-story, residential-over-retail or office-over-retail development along Main and Santa Clara Streets, adjacent to the Railroad Property to contribute to the day/night activity in the CBD.
- Provide a transition from the downtown core to the surrounding neighborhoods along Main Street and Santa Clara Avenue that is compatible with the tourist commercial uses on the Railroad Property.

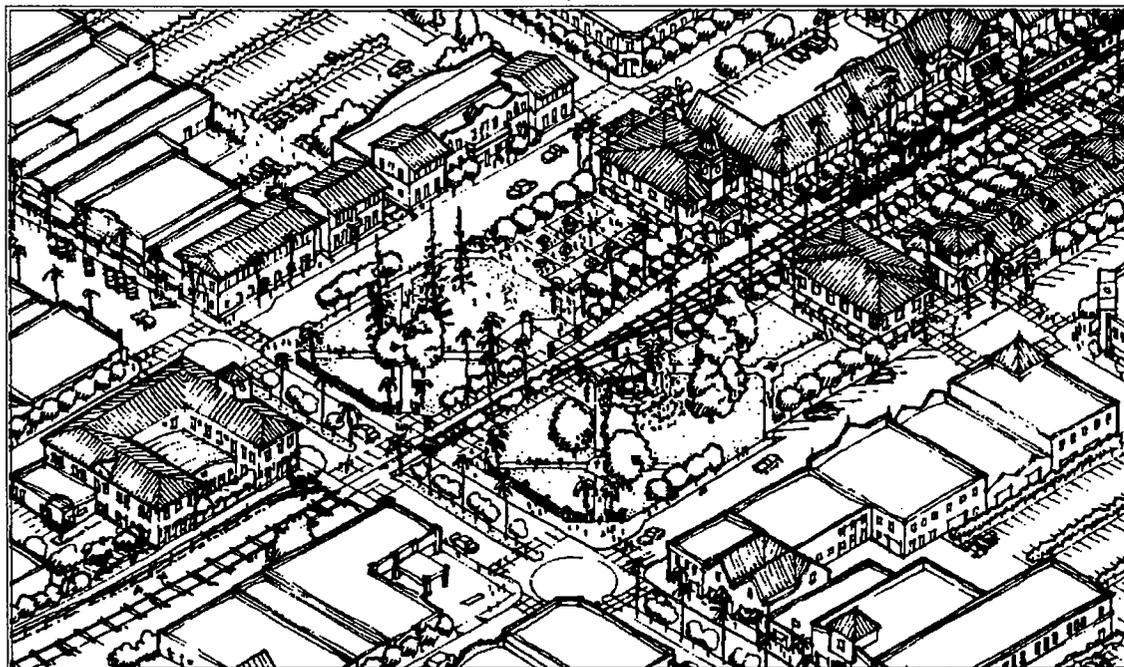
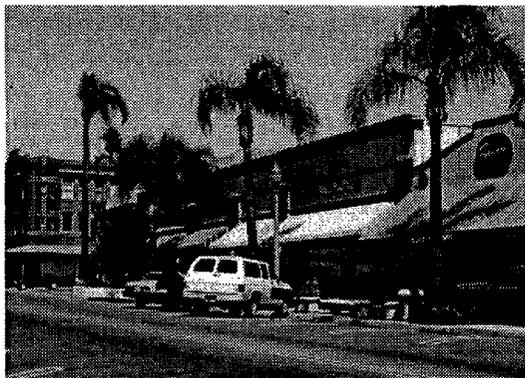




CBD LAND USE DESIGNATIONS, PERMITTED USES AND DEVELOPMENT POTENTIAL

The goals listed above are implemented through the General Plan and Zoning designation of Central Business District "CBD" which applies to all parcels within the CBD boundary.

The permitted uses and development standards are those outlined in the City Zoning Ordinance for the CBD zone district. This zone district promotes a pedestrian-oriented commercial environment by requiring ground floor retail uses, buildings that are located on the front property line and



CENTRAL PARK WILL BECOME THE TRUE CENTER OF DOWNTOWN FILLMORE.

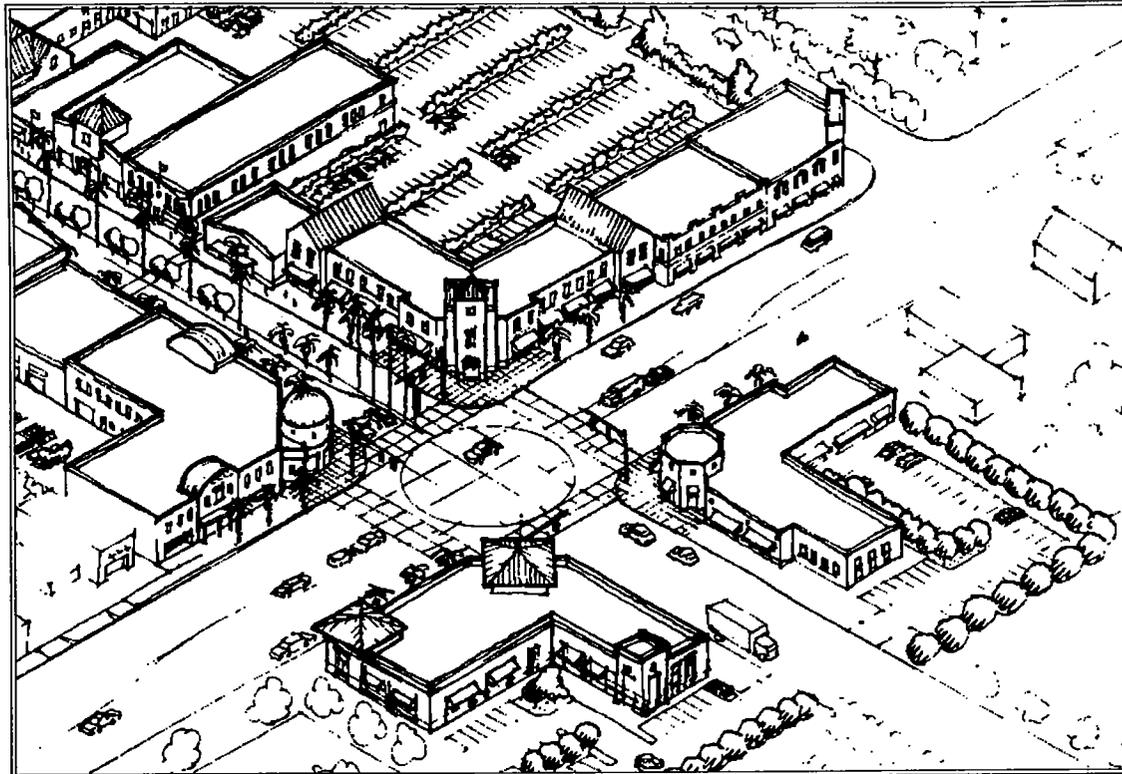
FIGURE 3.2

parking that is located to the rear of development, preferably in public parking lots.

A CBD Transition Area surrounds the Railroad Property. Main Street and Santa Clara Avenue from the west side of Saratoga Street to the west side of Mountain View Street should function like the downtown core but feel like a neighborhood. This area is differentiated to allow for a less formal separation of uses and a required front setback to provide for yards, landscaping, front porches/stoops which provides a residential character.

The new growth potential for parcels within the new CBD area has been calculated assuming that all underutilized and vacant parcels would build-out to the maximum two story potential as allowed in the CBD Zone. The new development potential for the CBD has been identified, and analyzed in depth in the EIR document. The predicted worst case new development potential is approximately 605,638 square feet.

This EIR document is available for review at the City's Community Development Department.



GATEWAY TO DOWNTOWN: THE INTERSECTION AT CENTRAL AVENUE AND HIGHWAY 126

FIGURE 3.3

CBD DEVELOPMENT POLICIES

The Development Policies listed in this section apply to all properties in the CBD, excluding the Railroad Property which has separate development polices. These policies are *mandated* standards that apply, in addition to the development standards contained within the CBD Zone of the City Zoning Ordinance.

1. Refurbishment and rehabilitation of existing buildings and development of new structures within the CBD shall be undertaken in conformance with Central Business District Architectural Design Guidelines established within this Plan. (See Chapter 4).
2. All new signs in the CBD shall be designed in accordance with the Central Business District Sign Design Guidelines established within this Plan. (See Chapter 4).



- 3. All new and renovated development shall conform with the Design Guidelines established within this Plan. (See Chapter 4).
- 4. Landscape or wall buffering of parking lot/loading areas shall be incorporated into project design to minimize aesthetic impacts.
- 5. For the security and safety of future users, development applicants and/or developers shall incorporate the following design concepts into site plans:

Circulation for pedestrians, vehicles, fire safety and police patrols;

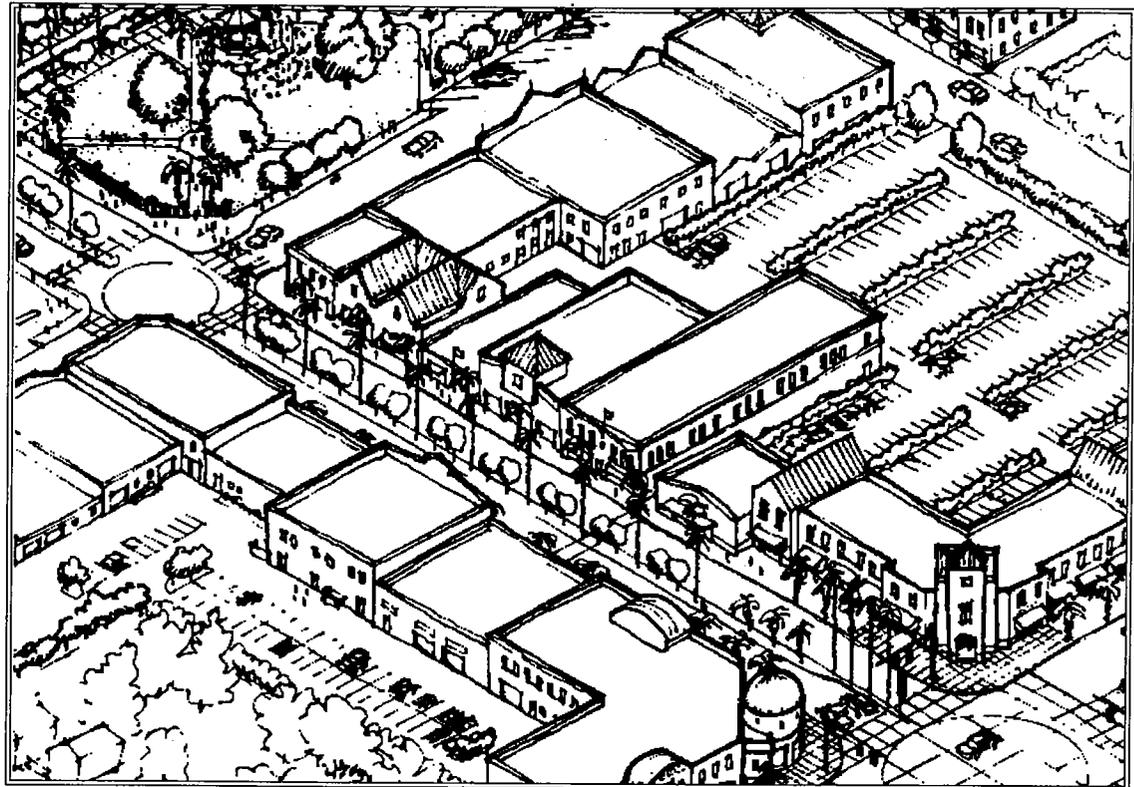
Lighting of streets, walkways, public or common areas;

Design of plazas, building entrances, pedestrian walkways, windows, fences and landscape areas in a manner that appropriately addresses security needs.

- 6. All utilities shall be installed underground, whenever economically and physically feasible.

CBD PHASING PLAN

The expansion and enhancement of Fillmore's Downtown as expressed in this Plan is anticipated to occur over a twenty year period. The enhancement of the CBD and development of the Railroad Property will require flexibility, as well as periodic review and monitoring. This flexibility may be required because

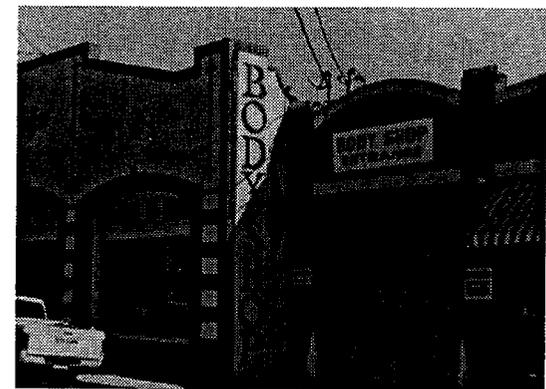


DEVELOPMENT IS ENCOURAGED ON CENTRAL AVENUE.

FIGURE 3:4

of changing market demands or modifications to planning strategy. Review and monitoring by the City, however, will ensure that progress is made in a logical and efficient manner that does not exceed financial resources or place undue stress on necessary infrastructure and facilities.

This phasing plan outlines an approach Fillmore should undertake to achieve the Plan goals. A high priority is the establishment of improvements to support the relationship between existing and future





CBD uses and the Railroad Property theme-related uses, followed by the construction of public improvements that will make a visible commitment to the enhancement of the CBD. These improvements will add value to the area, thereby laying a foundation for future private sector investment.

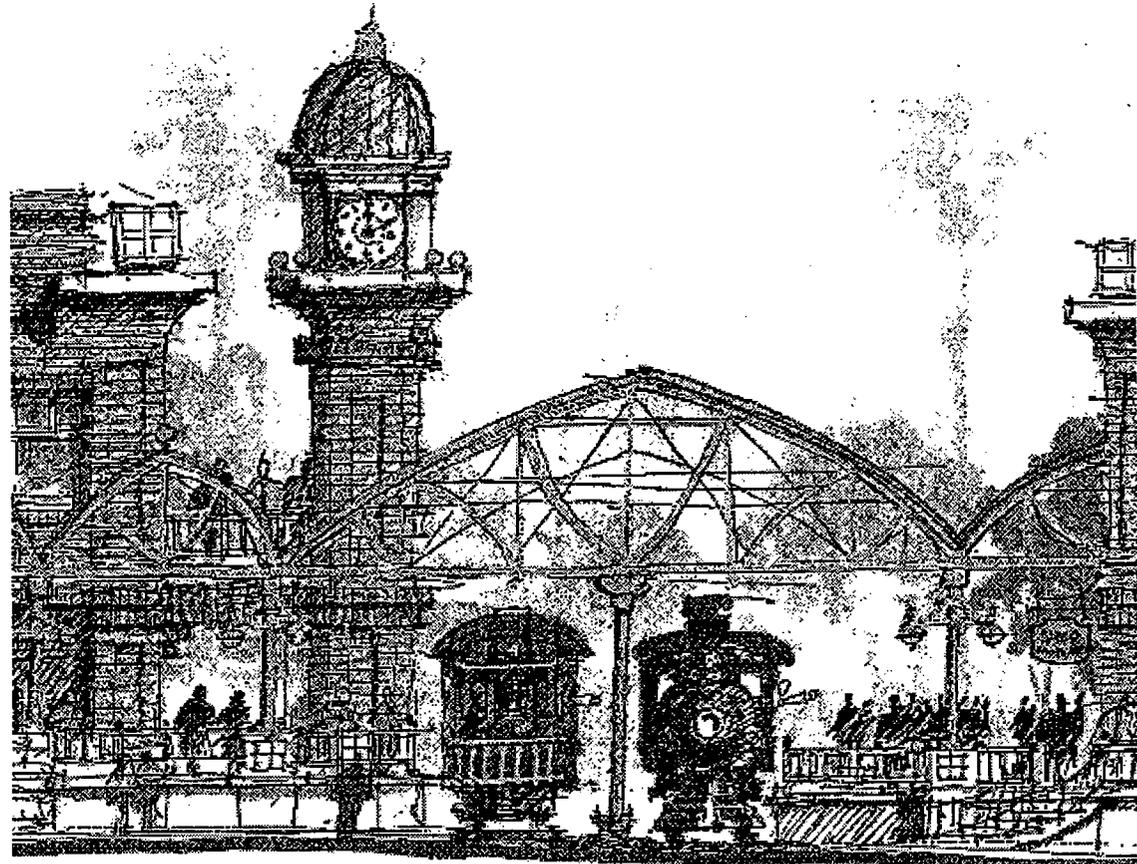
In consideration of these priorities and the current economic conditions, the following tasks should be addressed within the CBD in descending order of importance:

1. Support, as feasible, the initial operations of the depot museum and Shortline Enterprises.
2. Develop a gateway monument at the Central Avenue/Highway 126 intersection.
3. Enhance existing downtown parking facilities.
4. Acquire or otherwise facilitate the construction of additional downtown parking areas that will be necessary to enable maximum development of the Railroad Property and the CBD.
5. Facilitate the rehabilitation and redevelopment of the properties adjacent to the Railroad Property; and establish a program for the phased implementation of streetscape improvements within the CBD.
6. Facilitate the redevelopment of Central Avenue, and private properties fronting it, from Santa Clara Avenue to Highway 126.

7. Develop civic facilities adjacent to, or in the immediate vicinity of Central Park.

This generalized phasing plan is intended to assist the City in the application of its resources. It is not intended that developer applicants, wishing to

implement development that is in compliance with the goals of the Downtown Specific Plan, be unduly restricted by this phasing plan.



B

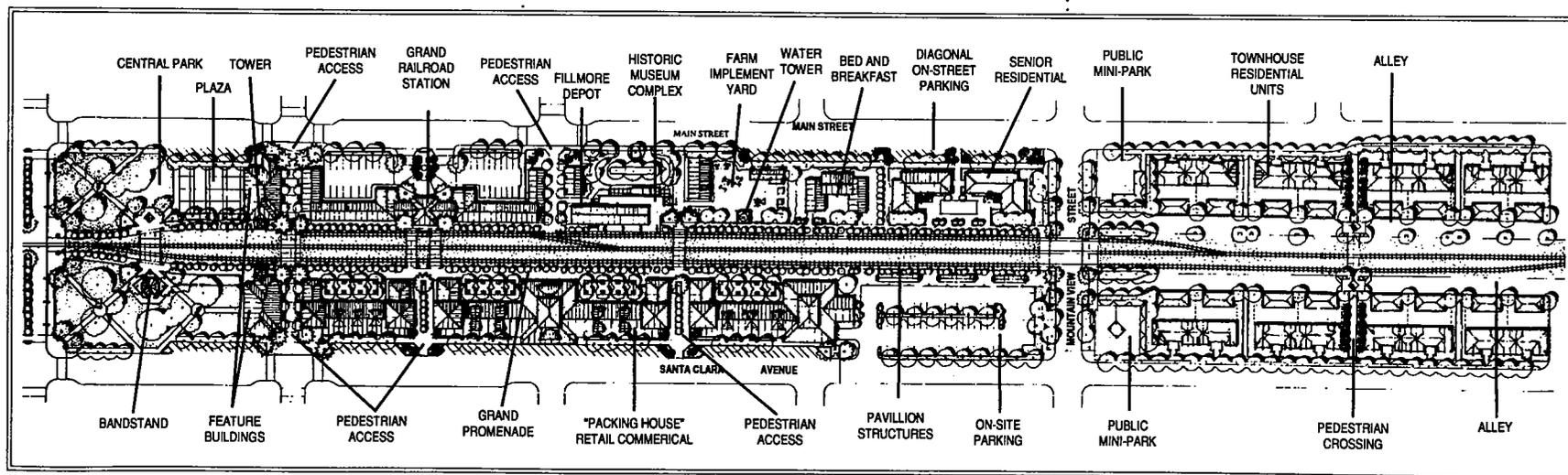
RAILROAD PROPERTY

The Railroad Property portion of this Plan encompasses approximately thirteen acres which border the railroad tracks and which run from east of Mountain View Street to Central Avenue. This property is bounded on the north by Main Street and on the south by Santa Clara Avenue as illustrated in Figure 3.4.

RAILROAD PROPERTY LAND USE GOALS

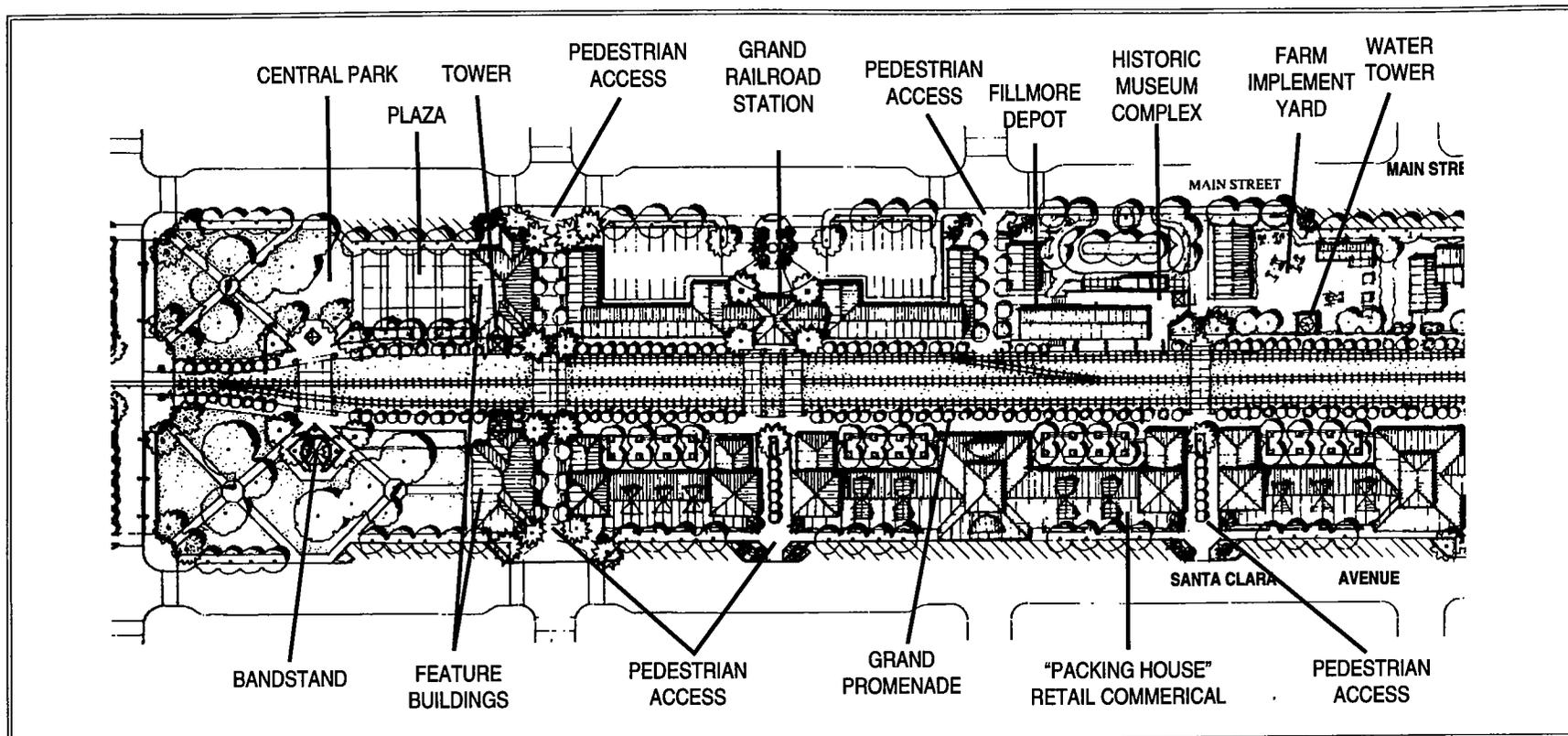
Under this Plan, this property is subject to distinctive goals, development polices and standards which differentiate the property from the Central Business District. The goals for this property are as follows:

- Implement a concept plan for future development of the Railroad Property which is very specific relative to uses, siting, densities and mix, but allows a certain level of flexibility to accommodate innovative applicant/developer approaches and changing economic and market conditions.
- Establish a comprehensive development program to be implemented by the City as the master developer "directing" a team of individual developers over a period of five to seven years or developed in a shorter time span through transfer of the property directly to a private developer/builder. This transfer could occur through a sale of the site or through a public/private partnership.
- Expand and enhance Central Park to become the town center, creating a true town square with open plaza space, park greens, shade trees and historic elements.
- Establish a dinner train attraction and other commercial uses within the Railroad Property to encourage tourism within the CBD.
- Enhance opportunities for movie industry use of the Railroad Property.
- Provide land for the development of a historical museum complex that could feature the re-



RAILROAD PROPERTY: CONCEPT PLAN

FIGURE 3.5



WESTERN PORTION OF RAILROAD PROPERTY

FIGURE 3:6

located and renovated railway depot.

- Develop a highly visible “promenade” along each side of the train tracks which establishes a celebrated gateway and a link from Central Avenue east through the new property to attract pedestrians, shoppers and diners into activity areas within the Railroad Property.
- Develop a grand railroad station on the property.

- Develop uses that encourage overnight trips to Fillmore.
- Create a community commons for a farmers market and festivals as an interim or long-term use.
- Provide an opportunity for the construction of residential units for seniors and/or first-time home buyers on the eastern portion of the Railroad Property.

- Establish architectural, sign and landscape guidelines to create a unique railroad-theme for new development.



RAILROAD PROPERTY CONCEPT PLAN

The goals listed above have been embodied in a "Concept Plan" for the Railroad Property. This Concept Plan reflects the community's desire for particular land uses, densities and their locations. This concept is meant to be directive but not mandatory. It is recognized that flexibility will be needed regarding building sizes, locations and uses. This concept plan will be implemented through the Development Policies contained in the next section as well as the Design Guidelines in Chapter 4. The Concept Plan contains the components described below.

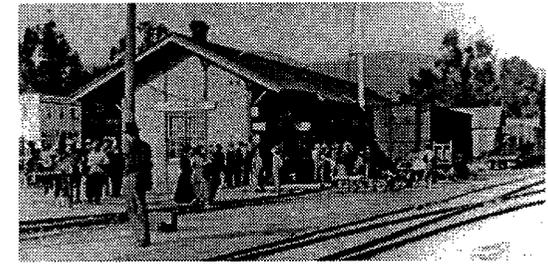
WESTERN PORTION OF RAILROAD PROPERTY:

- Central Park to be expanded to the north and east to approximately 1.4 acres, excluding railroad track right-of-way, with open plaza space, park greens, shade trees and historic elements.
- Two "feature" buildings of approximately 12,000 square feet each to be developed at the eastern edge of Central Park with architecture that is consistent with the CBD Architectural Design Guidelines. These buildings would serve as a gateway and entry features between the CBD and the Railroad Property. It is anticipated that these buildings will be two stories and house specialty retail and restaurant uses on the first floor with office uses above.
- A grand railroad station to be constructed on the north side of the tracks, consisting of about 12,000 square feet and featuring a waiting room, a ticket

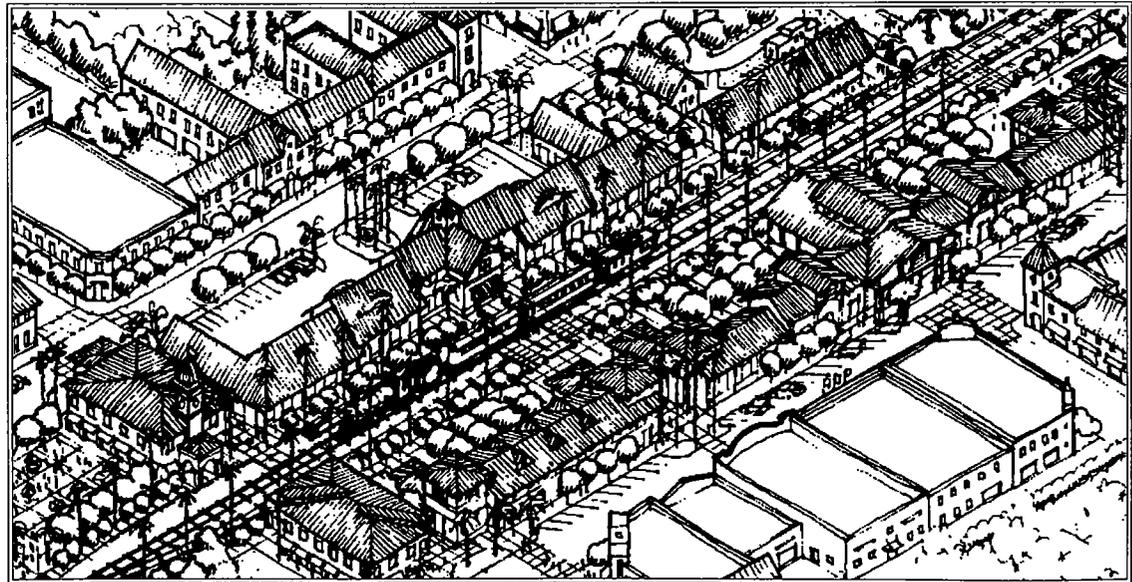
booth, exhibit space, a restaurant and a saloon. This use would support the dinner train.

- Promenade specialty retail uses would be developed in the area on the south side of the tracks between Fillmore and Clay Streets. This could include approximately 52,000 square feet of specialty retail, which could include an in-door farmer's market, arts and crafts sales and studios, boutique, a bookstore, a coffee shop, restaurants, a specialty market, or a performing arts theater of approximately 6,800 square feet. Approximately 10,000 square feet of this development could be second floor office use.

- A historical museum complex to be developed that could feature a relocated and renovated railway depot and other historic buildings along with a garden, fountain and/or plaza. This complex would utilize approximately one-half acre of land between Saratoga and Clay Streets. It is expected that the Fillmore Historical Society would own and operate the complex as a museum.



FILLMORE DEPOT ABOUT 1900.

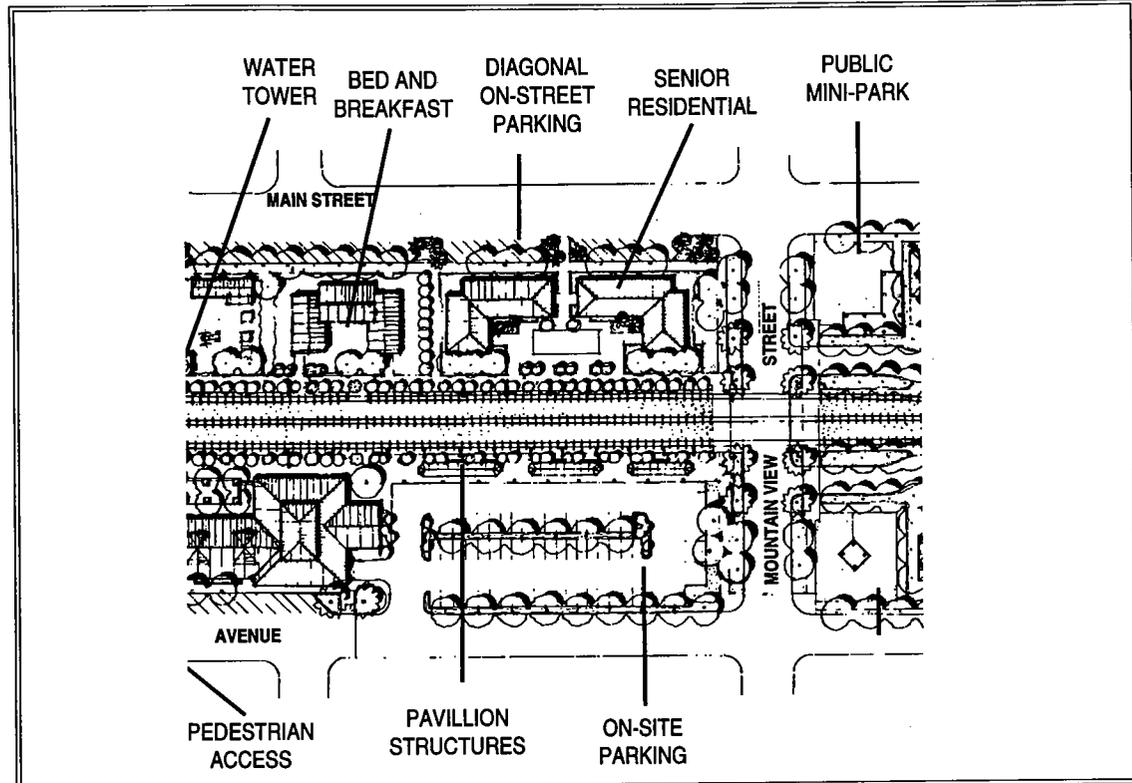
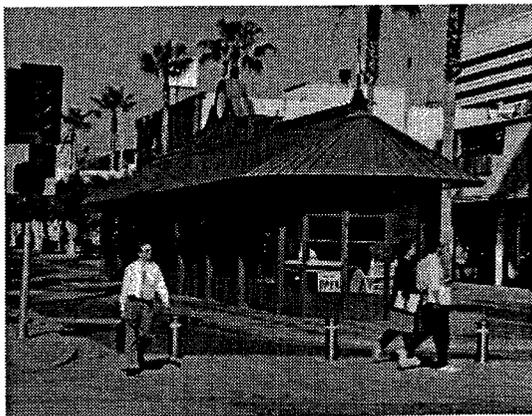


WESTERN PORTION OF RAILROAD PROPERTY THEME DEVELOPMENT



CENTRAL/TRANSITIONAL AREA OF RAILROAD PROPERTY:

- Hotel, inn or bed and breakfast uses of approximately 6,500 square feet to be developed fronting on Main Street. Also, up to 30 units of senior housing or 8 townhouse units can be developed in this area. There is the potential to locate a performing arts theater on the south side of the tracks on this central portion of the property. If a theater is developed, the need for additional parking to support the theater may reduce the number of residential units that could be built on the north side of the tracks. On the other hand, a total of 60 senior units or 16 townhomes may be constructed on either/both sides of the tracks, if retail development is reduced and no theater is developed.
- Another option for the north side of this portion of the property would be a Railroad Roundhouse and Turntable. This development would require approximately 0.9 acres and would take the place



CENTRAL/TRANSITIONAL AREA OF RAILROAD PROPERTY

FIGURE 3.7

of the hotel and senior residential units along Main Street. In this option, the hotel could move to the south side of the tracks, along Santa Clara Avenue, as the parking lot would be moved to the north, to be incorporated into the Roundhouse development. The senior housing could be built at another location on the eastern or southern side of the Railroad Property or elsewhere in the CBD.

The Roundhouse facility is not intended for use as a general maintenance facility for the trains, but rather is intended to support the tourist, historic and film industry uses of the Railroad Property development. The Roundhouse facility could involve the following uses for which admission may be charged:

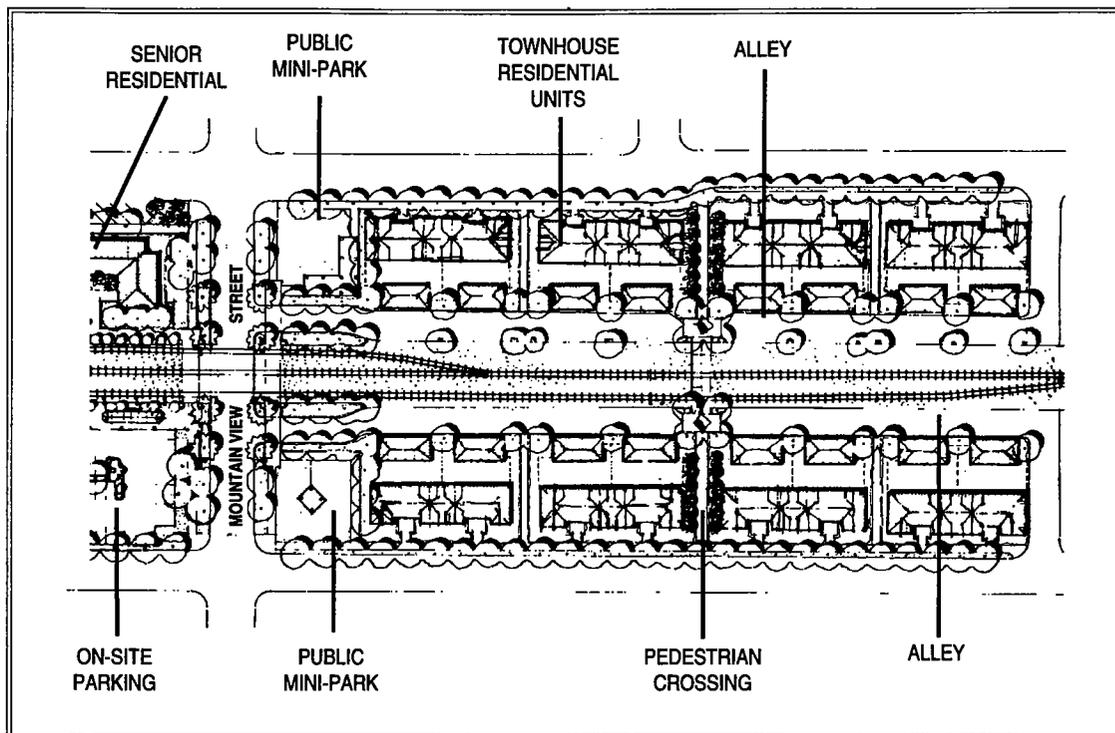
1. Display of trains and railroad artifacts.
2. Sound stage and movie set for film companies.



- 3. Display of travelling museum exhibits.
- 4. Meeting room for community gatherings or conferences.

The Roundhouse would be approximately 5,000 square feet in size and contain four stalls that are approximately 100 feet deep. The Turntable would be approximately 90 feet in diameter.

- A parking lot with approximately 70 parking spaces, in combination with pavillion structures adjacent to the tracks to support a farmer's market or other similar events, to be provided at the corner of Santa Clara and Mountain View Street. This plan anticipates that both on-site and off-site parking areas must be provided to support the new development on the Railroad Property. Parking requirements will consider conjunctive use between various on-site uses and off-site CBD uses.
- Interim or temporary uses are envisioned for the main on-site parking lot or for those areas not already developed or under construction. Such uses include, but are not limited to, a farmer's market, a staging ground for civic events, a display area for Shortline rail cars and/or movie filming.
- The westerly frontage of Mountain View to be improved to compliment the park on the easterly side. This will create an attractive and green entry into the area and the neighborhood to the north.



EASTERN PORTION OF RAILROAD PROPERTY

FIGURE 3.8

EASTERN PORTION OF RAILROAD PROPERTY:

- Up to 32 townhomes are planned for the 3.7 acres of Railroad Property located east of Mountain View Street.
- A "mini-park" or common area is envisioned on the east side of Mountain View Street adjacent to the street, to complement the park area in the commercial development. This open space area would serve as a buffer between commercial and residential uses and would serve as a residential entry point.

RAILROAD PROPERTY LAND USE DESIGNATIONS, PERMITTED USES AND DEVELOPMENT POTENTIAL

The General Plan and Zoning designations for the Railroad Property are "Central Business District (CBD)". The permitted uses and development standards are those that are listed in the City CBD Zoning Ordinance.

The development potential of the Railroad Property is subject to the following limitations; however a 10 percent increase is allowed, as approved by the City:



1. 88,000 square feet of railroad station, specialty retail, office, restaurant and entertainment uses.
2. Three quarters of an acre of museum/historical uses.
3. 6,500 square feet of lodging/bed and breakfast uses.
4. A 5,000 square foot Railroad Roundhouse and associated Railroad Turntable.
5. Residential density shall not exceed 15 units per acre unless bonus density is granted per the Zoning Ordinance standards.

RAILROAD PROPERTY DEVELOPMENT POLICIES

The policies listed in this section of the Plan are mandatory for all development on the Railroad Property. If it is desired to modify the *mandatory* policies, a Specific Plan Amendment would be necessary.

COMMERCIAL DEVELOPMENTS:

1. No setback or build-to lines shall be applicable on the Railroad Property west of Mountain View Street, except as noted below.

2. For the two feature buildings adjacent to Central Park, the westerly "extension" of Fillmore Street shall be the easterly build-to-line requirement.
3. A tower element shall be incorporated into the design of the building located adjacent to Central Park, on the north side of the railroad tracks.
4. The "promenade" along both sides of the railroad tracks shall be a minimum of fifteen feet in width. No permanent buildings, walls or fences, other than protective rails, shall encroach within the fifteen feet, except the tower of the north "feature building", constructed over the promenade with a pedestrian pass-through providing no less than 10 feet clearance.
5. The pedestrian promenade shall either be dedicated public right-of-way or an easement shall be recorded to provide for public use and reciprocal access for all developments along the promenade.
6. Development on the Railroad Property shall emphasize commercial orientation to the promenade at the railroad tracks and access to the promenade. Additionally, development on the Railroad Property shall emphasize orientation to the CBD by providing pedestrian ac-



CENTRAL AVENUE'S WEST SIDE, ABOUT 1910.



cess from surrounding public streets as indicated on the Concept Plan. Some commercial orientation to the public streets is encouraged, in the form of signs, display windows and occasional points of entry in order to assure a welcome and inviting appearance at the development's public street frontage.

7. Service entrances and loading areas shall be screened from views of the public street and the promenade.
8. Any structure built on the south side of the railroad tracks, adjacent to Saratoga Street shall have a prominent "central roof line feature" in-line with Saratoga Street.
9. A public view corridor into the Railroad Property shall be maintained on the north side of the railroad tracks at Saratoga Street.
10. Pedestrian access points shall be provided on the south side of the railroad tracks, at mid block between Fillmore and Saratoga Streets and between Saratoga and Clay Street. The minimum width for these pedestrian access ways shall be ten feet and the maximum width shall be twenty feet.
11. A parking lot accomodating about 70 cars shall be located on the south side of the railroad tracks, near the corner of Santa Clara Avenue and Mountain View Street.



CENTRAL AVENUE'S WEST SIDE, EARLY 1920'S.

12. Park or commons features shall be a required element of new development adjacent to Mountain View Street.
13. Retail specialty shops including restaurants shall be developed on both sides of the train tracks, along the promenade.
14. The historic railway depot, if relocated, shall be placed to the east of the proposed new railroad station.
15. Train maintenance activities, other than routine daily up-keep or filming related cosmetic modifications, are prohibited in any railroad related developments such as a round house or turntable.
16. For interim uses, all non-developed areas of the site shall be covered in turf or other appropriate ground cover and properly irrigated. Interim uses must be maintained and properly controlled on a regular basis. Applications for



interim uses shall be made to the Community Development Department, which shall determine the appropriateness of the use; the term for such use and the conditions under which the use may operate on site.

17. Development on the Railroad Property shall be constructed in conformance with the Railroad Property Architectural Design Guidelines. (See Chapter 4).
18. All signs on the Railroad Property shall be in conformance with the Railroad Property Sign Guidelines. (See Chapter 4).
19. All landscaping and paving shall be in conformance with the Railroad Property Landscape Design Guidelines. (See Chapter 4).
20. Development of public gathering places, plazas, sitting areas and areas for public art shall be included in all new developments.
21. Perimeter and interior landscaping shall be provided to buffer views of parking lot/loading areas to minimize aesthetic impacts.
22. For the security and safety of future users, new developments shall incorporate the following:

Balanced circulation for pedestrians, vehicles, fire safety and police patrols;

Lighting of streets, walkways, public or common areas;

Design of plazas, building entrances, pedestrian walkways, windows, fences and landscape areas in a manner that appropriately addresses security needs.

23. Common areas shall be owned and maintained by an association(s), whose responsibility shall be the maintenance of all common areas, circulation systems and landscaped areas. Such association(s) would be similar in nature to a condominium homeowners association and may be public or private.
24. All new and existing utilities shall be placed underground during project construction,

whenever economically and physically feasible.

RESIDENTIAL DEVELOPMENTS:

1. Townhouse development shall be substantially similar to that illustrated in the Conceptual Plan, Figure 3.8. Development of a differing housing type, such as stacked flats or single family homes, shall not be permitted unless approved by the City through an amendment to this plan.
2. The inclusion of rear-facing garages with access from alleyways is required in order to increase sound attenuation and meet design goals.



A CALIFORNIA RAILROAD BUILDING, LATE 1800'S.



3. Park or commons features shall be a required element of development adjacent to Mountain View Street.

4. For the security and safety of future users, new developments shall incorporate the following:

Balanced circulation for pedestrians, vehicles, fire safety and police patrols;

Lighting of streets, walkways, public or common areas;

Design of plazas, building entrances, pedestrian walkways, windows, fences and landscape areas in a manner that appropriately addresses security needs.

5. All new and existing utilities shall be installed underground during project construction, whenever economically and physically feasible.

RAILROAD PROPERTY PHASING PLAN

The expansion and enhancement of Fillmore's Downtown as expressed in this Plan is anticipated to occur over a twenty-to thirty-year period. The first phase of development is intended to occur within the Railroad Property as described below.

An undertaking of the magnitude proposed by this Plan will require flexibility, as well as periodic review and monitoring. Flexibility is required be-

cause of changing market demands or modifications to planning strategy. Review and monitoring by the City, however, will ensure that progress is made in a logical and efficient manner that does not exceed financial resources or place undue stress on necessary infrastructure and facilities.

This generalized phasing plan is intended to assist the City in the application of its resources. It is not intended that developer applicants, wishing to implement development that is in compliance with the goals of the Downtown Plan, be unduly restricted by this phasing plan.

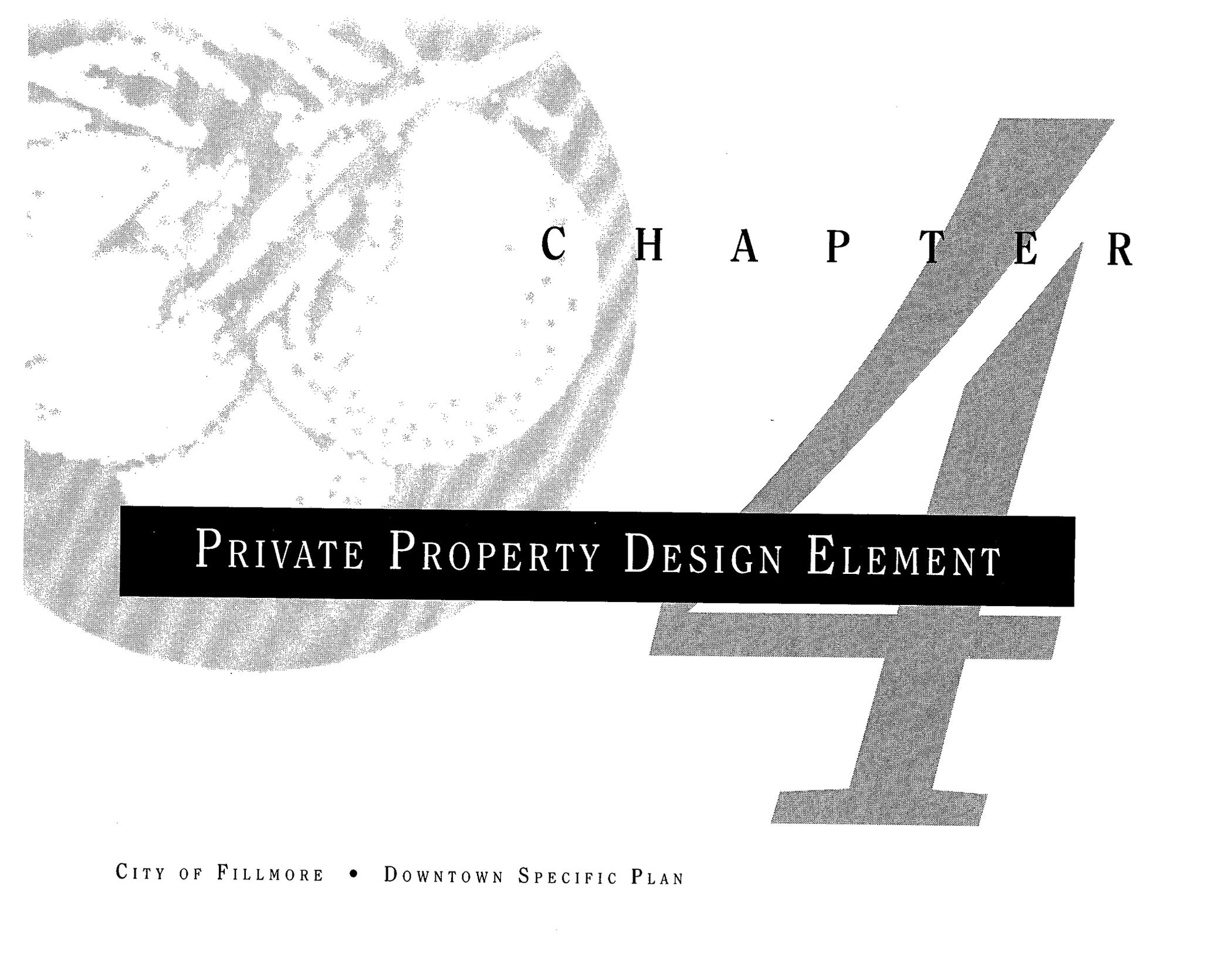
In consideration of current economic conditions, the City's resources should be directed to the following tasks in descending order of importance:

1. Relocate the Old Depot Building to the Railroad Property.
2. Support the initial operations of the museum and Shortline Enterprises on the Railroad Property.
3. Expand and redevelop Central Park.
4. Acquire or otherwise facilitate the construction of downtown parking areas that will be necessary to enable maximum development of the Railroad Property.
5. In conjunction with No. 3 above, initiate development of the Railroad Property. It is in-

tended that the Railroad Property be developed in three distinct phases over a five to seven year period as follows:

- The first phase should consist of Central Park and initiation of the Grand Railroad Station and historical complex, including the two feature buildings at the eastern edge of the park. This would provide a cohesive first phase development that connects Central Avenue to the Railroad Property. This would also establish Central Park as the western gateway to the Railroad Property development and the museum complex as the eastern anchor.
- The second phase should be development of housing east of Mountain View Street. Given market forces, it is possible that this housing could be developed first.
- The central portion of the property would be developed as a third or last phase and would include a performing arts theater, specialty retail, and a hotel/bed and breakfast inn. Although housing is allowed on this portion of the Railroad Property, it is not a preferred use and is intended to be constructed, if at all, as a final component.

Although the phasing plan for the Railroad Property should be allowed to respond to market conditions, it is intended that the three phases be developed in the recommended order.



C H A P T E R

PRIVATE PROPERTY DESIGN ELEMENT



INTRODUCTION AND PURPOSE

The Design Guidelines presented in this Plan have been prepared to address the Central Business District and the Railroad Property. In general, these design guidelines are intended to inform, guide and inspire property development and redevelopment within the Specific Plan area. A basic intent is to encourage good design that is compatible with the existing character of Fillmore as “the Last Best Small Town In Southern California”.

A second fundamental intent is to provide property owners, developers and design consultants with guidelines that meet City standards and expectations. It is not the intent of these guidelines to severely restrict individual preferences, ignore economic dictates relative to materials and style, or discourage innovative design or individual expressions.

All proposed structural or facade changes as well as new public or private construction in the Specific Plan area that require the issuance of a Building Permit, are required to be consistent with the Design Guidelines presented in this Plan. It is also required that a Design Professional prepare the plans for facade changes and renovations. A licensed architect is required to prepare plans for all new construction within the Specific Plan area. The process for design review is to be in compliance with the Design Review requirements in the City’s Zoning Ordinance.



CENTRAL BUSINESS DISTRICT

CHARACTER AND SCALE OF THE CENTRAL BUSINESS DISTRICT CORE AREA

The area comprising Fillmore’s Central Business District (CBD) core area was established as the original business core prior to the turn of the century. Originally, the built character of the area was one of healthy commerce, primarily supporting the agriculturally based economy, with a variety of shops, warehouses, banks, mercantiles, and residences emanating from the site of the Southern Pacific Railroad Depot.

The downtown experienced its greatest transition during the 1910’s and 1920’s as investment and a favorable business climate encouraged construction of a reasonably high caliber throughout the downtown core. Today, except for minor remodeling and storefront alterations, and the addition of public sidewalk and street improvements, the downtown remains virtually unchanged. With rare exception, most of the



TYPICAL CENTRAL AVENUE FACADE, 1993



TYPICAL CENTRAL AVENUE BUILDINGS, 1993

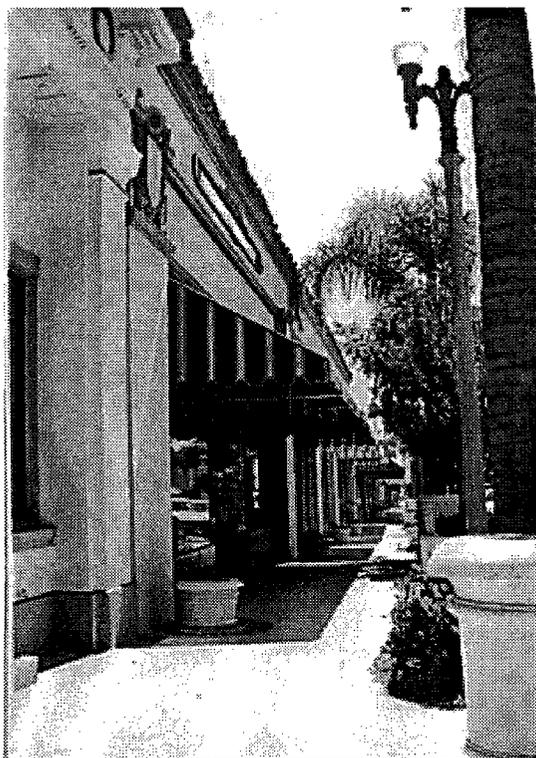
buildings are low-scale, one and two story brick masonry structures on small lots of twenty-five to fifty feet in width. These buildings emphasize their individual storefronts, creating, in effect, a commercial stage set - in which simple rows of box-shaped buildings present grand facades of masonry, tile or stucco, adorned with classic or traditional details, and typically reserving a central or prominent portion of the building fronts for signage. It is the imagery and early twentieth century character of these buildings upon which the Central Business District Design Guidelines are based.



THE PRIMARY DESIGN OBJECTIVES OF THESE DESIGN GUIDELINES ARE AS FOLLOWS:

I. PROMOTE THE IMAGE OF "SMALL-TOWN AMERICA'S MAIN STREET"

Downtown Fillmore can be likened to the classic American small town, particularly as it flourished during the 1910's and 1920's. These guidelines seek to promote and protect this "small town" character within the CBD, establishing design criteria which will allow for the



possibility of more intense development while assuring a relatedness between individual buildings, both new and old.

To this end these guidelines identify the following architectural styles *as appropriate sources of inspiration* for new development within the CBD core area:

- Early 20th Century Civic-Commercial (fig. 4.1)*
- Italianate (fig 4.2)
- Neo-classical (fig. 4.3)
- Spanish Colonial (fig. 4.4)
- Art Deco (fig. 4.5)

* *This style is similar to the "Railroad Theme" identified by the City of Fillmore's Ventura Street Design Guidelines.*

II. PROVIDE PEDESTRIAN ORIENTED DESIGN

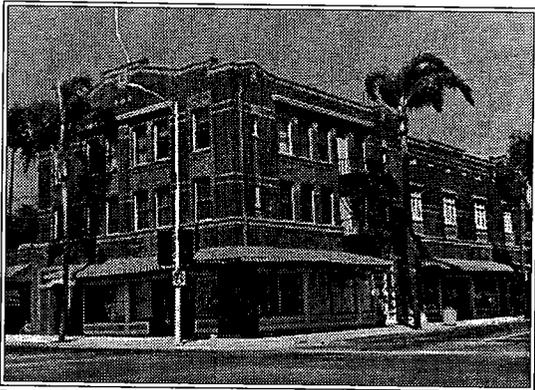
These guidelines reinforce the urban context of downtown Fillmore. Care will be taken to assure that new and renovated projects present the best possible pedestrian environment at street level. These guidelines therefore address street level building design elements which specifically enrich the pedestrian experience and convey the image of high quality urban architecture in a manner compatible with the existing character of the CBD.

III. MANDATE COMMERCIAL FRONTAGE IN THE CBD CORE AREA

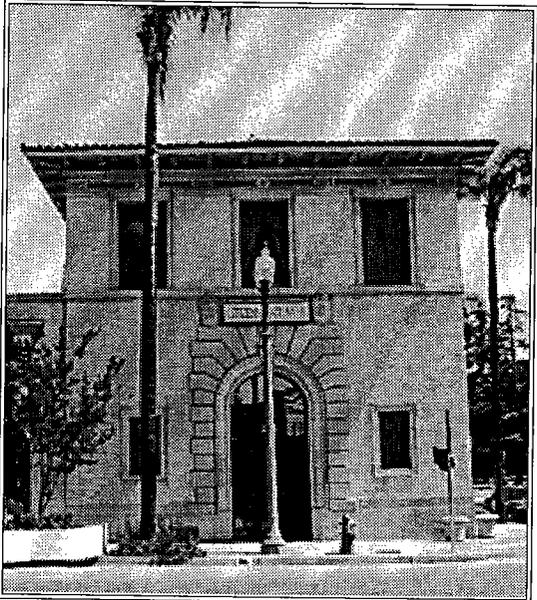
Commercial buildings should address the street in a manner which supports and encourages an interaction of building tenants and pedestrians. The building design should allow for individual expression of separate shops, including variation in storefront designs within the same building, window displays and signage, while assuring a degree of conformity in keeping with the building's overall design theme.

IV. PROVIDE COMPATIBILITY BETWEEN THE CBD CORE AND SURROUNDING NEIGHBORHOODS.

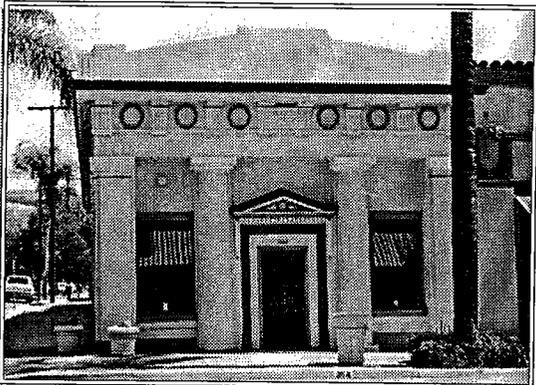
A transitional area is created along Main Street and Santa Clara Avenue across from the Railroad Property. Architecture in this transitional area is to be residential in character, reminiscent of those styles found throughout Fillmore and Southern California at the turn of the century. Building design should incorporate traditional elements such as broad eaves, chimneys, stoops, porches, bay windows, and dormer windows.



EARLY 20TH CENTURY CIVIC-COMMERICAL (DESTROYED 1994) FIGURE



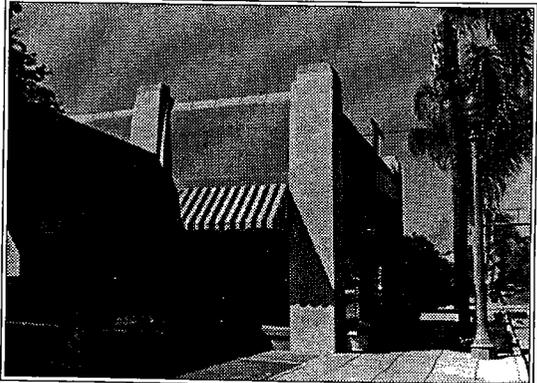
ITALIANATE FIGURE 4.2



NEO-CLASSICAL FIGURE 4.3



SPANISH COLONIAL FIGURE 4.4



ART DECO FIGURE 4.5

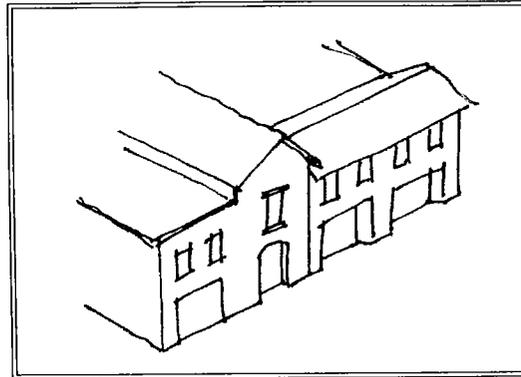


CENTRAL BUSINESS DISTRICT CORE AREA BUILDING DESIGN GUIDLINES

To accomplish the above objectives, these guidelines establish specific design criteria which are intended to guide and direct development within the Central Business District core area. Guidelines for the transitional area surrounding the railroad property are located at the end of this section of the Plan.

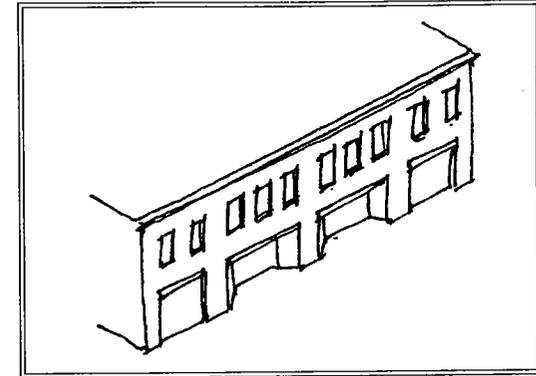
1. BUILDING MASS AND ORGANIZATION

To assure that new and renovated buildings are compatible with the existing character and scale of the CBD, buildings should be organized into increments of no less than twenty-five (25) feet and no more than fifty (50) feet in width (the historic parcel increments typically found in the CBD are 25 ft. or 50 ft. in width). Buildings which occupy more than fifty feet of frontage should be designed to appear as several small buildings or several smaller but related parts of a larger structure. This can be accomplished in several ways, such as: incremental changes in roof eave lines and/or ridge lines (fig.4.6); changes in wall plane (fig 4.7); grouping of windows into varying or repetitious patterns (fig.4.8); and coordinated placement of column elements, walls, wall openings and storefronts (fig. 4.9). In addition, the following criteria should be addressed:



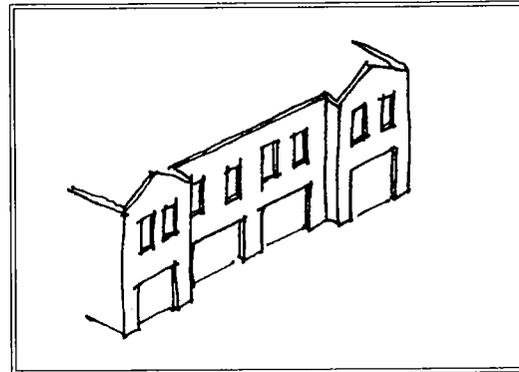
INCREMENTAL CHANGES IN ROOF EAVE LINE.

FIGURE 4.6



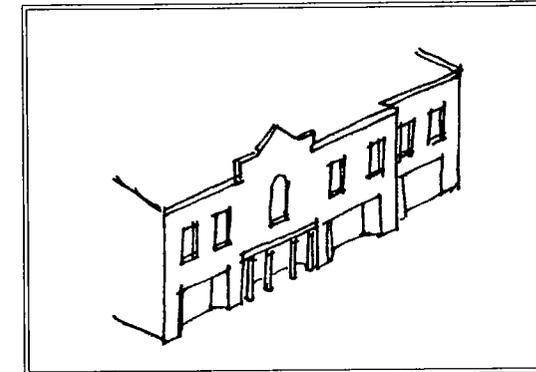
GROUPING OF WINDOWS.

FIGURE 4.8



CHANGES IN WALL PLANE.

FIGURE 4.7



PLACEMENT OF COLUMN ELEMENTS, WALLS, OPENINGS, ETC.

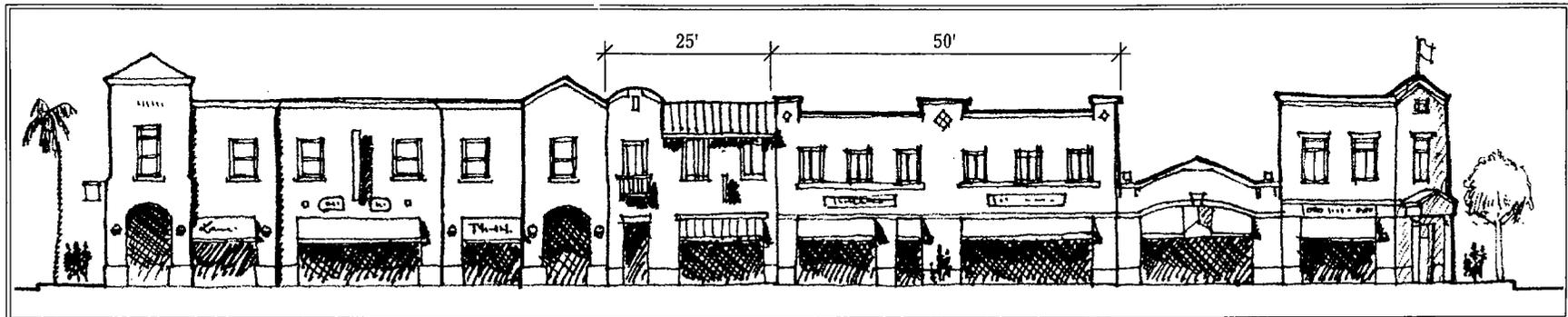
FIGURE 4.9

A. STREET LEVEL BUILDING INCREMENT (FIG.4.10)

At street level, building design should reflect the regular and somewhat uniform pattern of alternating facades (we will call this the "building increment"). Each building is made up of a combination of entries and storefronts, interrupted by decorative columns or brief structural walls which serve to define the end of one shop or building and the beginning of another. Historically, the building increment in the CBD was inherently reflective of the structural

systems within. The limitations of those systems were quite compatible with the small lot - small building development that is so familiar in downtowns of this period.

However, today because of technical advances (increased strengths and longer spans of structural steel) in modern building systems, there may not be an inherent correlation between the pattern and placement of actual structural components and the desired pattern and placement of design elements,



NEW AND RENOVATED BUILDINGS SHOULD EXPRESS FILLMORE'S HISTORIC STREET-LEVEL BUILDING INCREMENT OF 25 TO 50 FEET.

FIGURE 4.10

such as walls and columns, as described in these guidelines. To support the objectives of these guidelines, exterior building design goals should, in all cases, take precedence over engineering preferences:

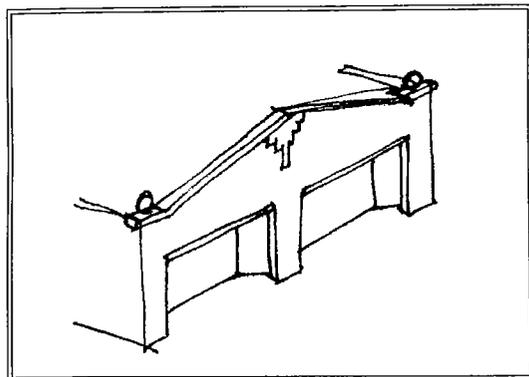
1. Building increments should be a maximum of fifty (50) feet in width, but should typically vary between twenty-five (25) and fifty (50) feet (the historic parcel sizes within the CBD). Buildings which exceed fifty (50) feet of frontage should use the architectural devices described herein to reinforce the building increment and pattern of the existing CBD.
 - a. At least one building entrance should be provided every fifty (50) feet, although more points of access are highly recommended, as they encourage a higher level of pedestrian activity at the street.
 - b. Buildings should express the historic increment through the use of vertical design elements (columns, pilasters, etc.) approximately every twenty-five (25) feet.

2. Storefront openings (the space or opening between columns or other solid building mass, in which storefront windows and/or doors are located) should be a maximum of twenty (20) feet in width. Storefront systems within these openings should be designed in substantial conformance with Section 3, of these guidelines.

B. ROOF AND UPPER LEVEL BUILDING INCREMENT ARTICULATION

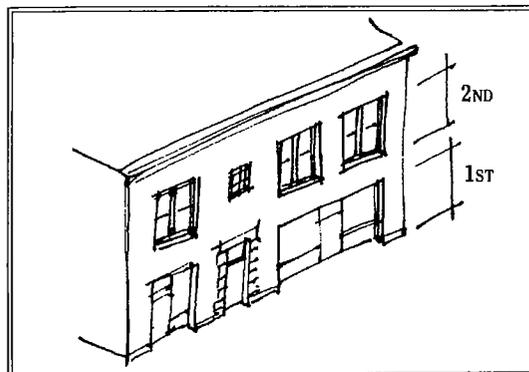
Buildings within the CBD are typically quite simple in mass and form, and the roof lines of such buildings reflect this simplicity. New buildings with lot frontage of fifty feet or less should present a profile expressive of this simplicity. Buildings with frontage which exceeds fifty (50) feet may incorporate changes in roof form which serve to reinforce the expression of the historic building increment, as defined in Item 1 A above. In addition, the following criteria should apply.

1. Single story buildings should provide an interesting and distinguished "roofline" or profile, as through the use of a decorative parapet (fig.4.11).
 2. The upper portion of two story buildings should be designed to reflect a refinement of the increment articulation found at the street level of the building facade.
 - a. The primary components at the upper story, such as windows, wall panels, projecting bays, etc., should typically be grouped in a manner consistent with the building increment established at street level (fig. 4.12).
 3. Cornices and molding should be used to unify and connect the composition of the building facade (fig.4.13).
- #### C. SPECIAL ARCHITECTURAL FEATURES
1. Highly prominent architectural features, such as towers and turrets should be reserved for buildings occupying key locations within the CBD, such



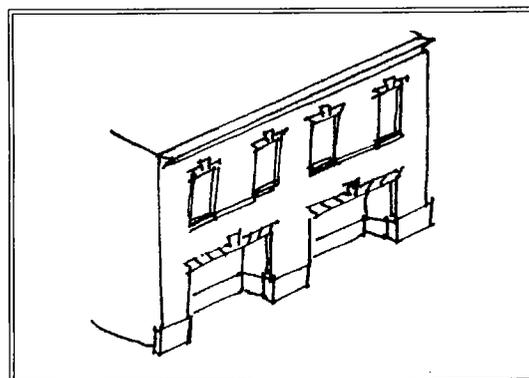
DECORATIVE PARAPET

FIGURE 4.11



UPPER STORY COMPONENTS RELATE TO STREET LEVEL

FIGURE 4.12



CORNICES AND MOULDING

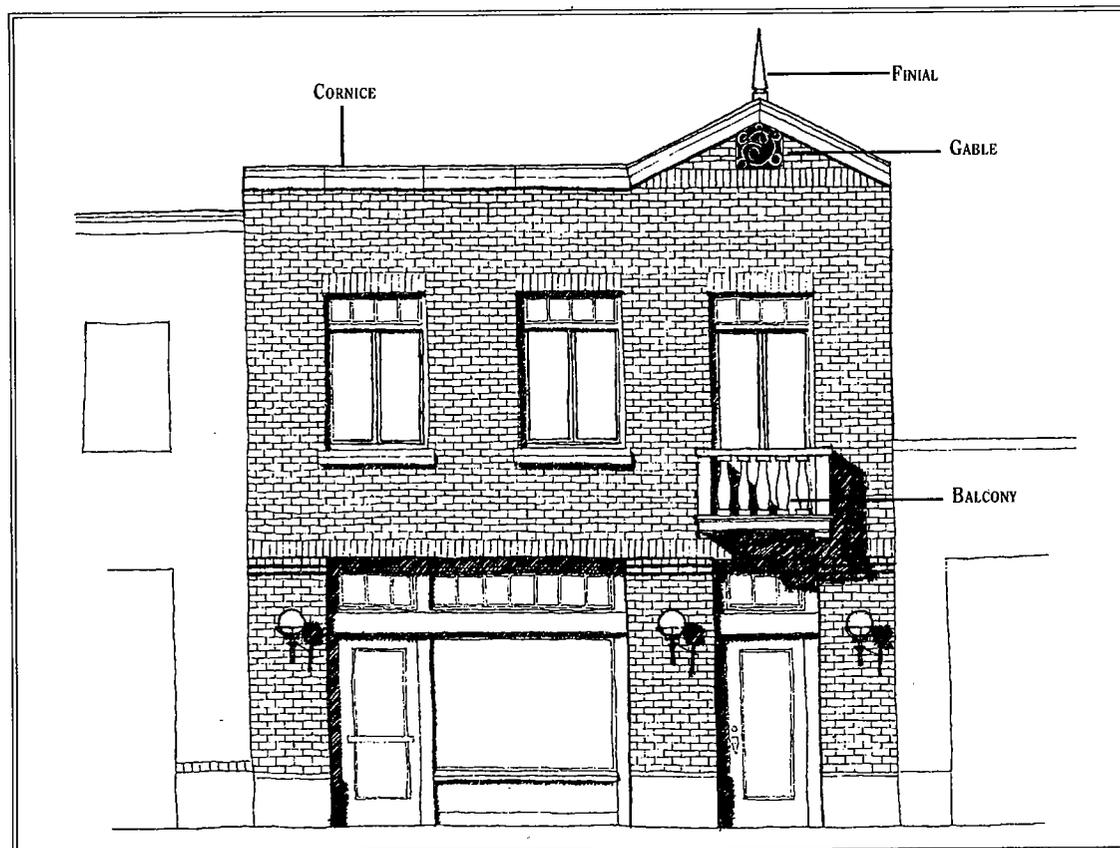
FIGURE 4.13

as street corners or at the point of connection for a mid-block pedestrian alley or court.

2. Features, such as gables, bay windows, balconies, finials, etc. are encouraged where deemed appropriate to the character of the building design (fig. 4.14).

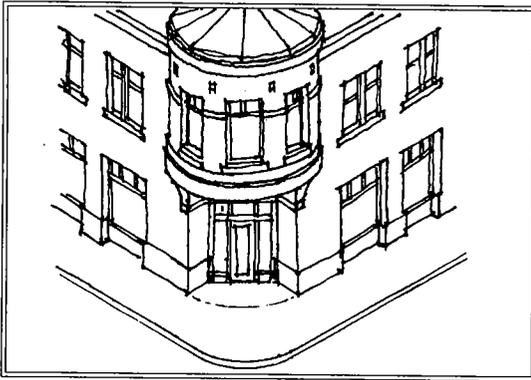
D. CORNER CONDITIONS

1. Street corners provide a unique opportunity to announce a building's presence and establish a memorable point of reference. Building configuration should respond in one of several basic ways, as illustrated here (Fig. 4.15 a, b, and c). Building entries should be designed in a manner consistent with "Corner Entries", as described below.



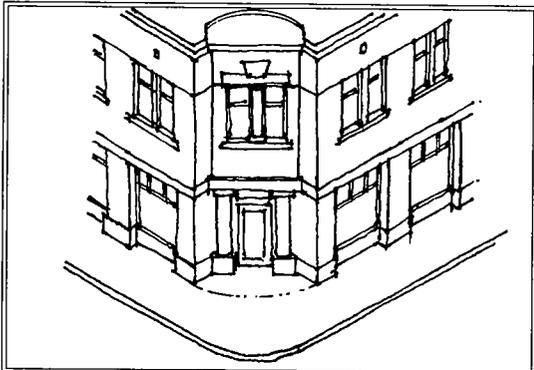
GABLES, BAY WINDOWS, BALCONIES, FINIALS, ETC. ARE ENCOURAGED WHERE DEEMED APPROPRIATE TO THE BUILDING DESIGN.

FIGURE 4.14



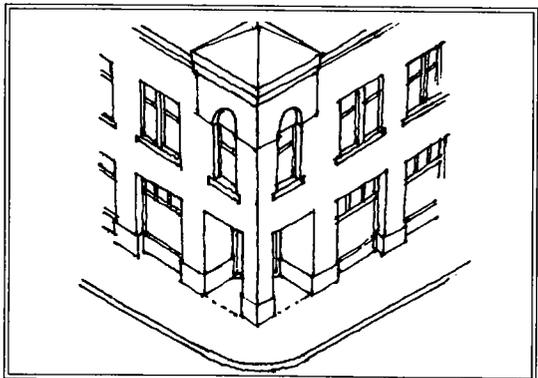
CORNER TURRET

FIGURE 4.15A



CUT CORNER

FIGURE 4.15B



CORNER PASS-THROUGH

FIGURE 4.15c

E. IN-FILL PROJECTS

1. In-fill projects should maintain the general character of the surrounding existing development, responding sympathetically to adjacent architectural character and details where appropriate and in keeping with these guidelines.

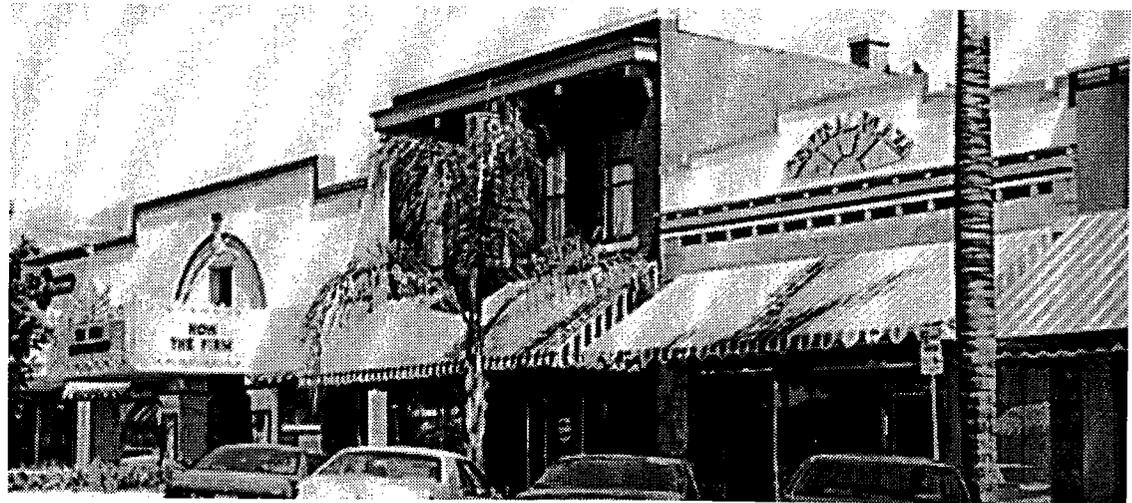
2. BUILDING FACADE COMPOSITION

The Facade of a building is the portion which faces onto the street, and it can be thought of as a public "stage set". It is the combination of a building's various parts - walls, columns, windows, store-fronts, parapets, etc., into a proportionate and rhythmically related whole, which serves to create the total composition of a building facade. It is the comparative proportions and rhythms, from one

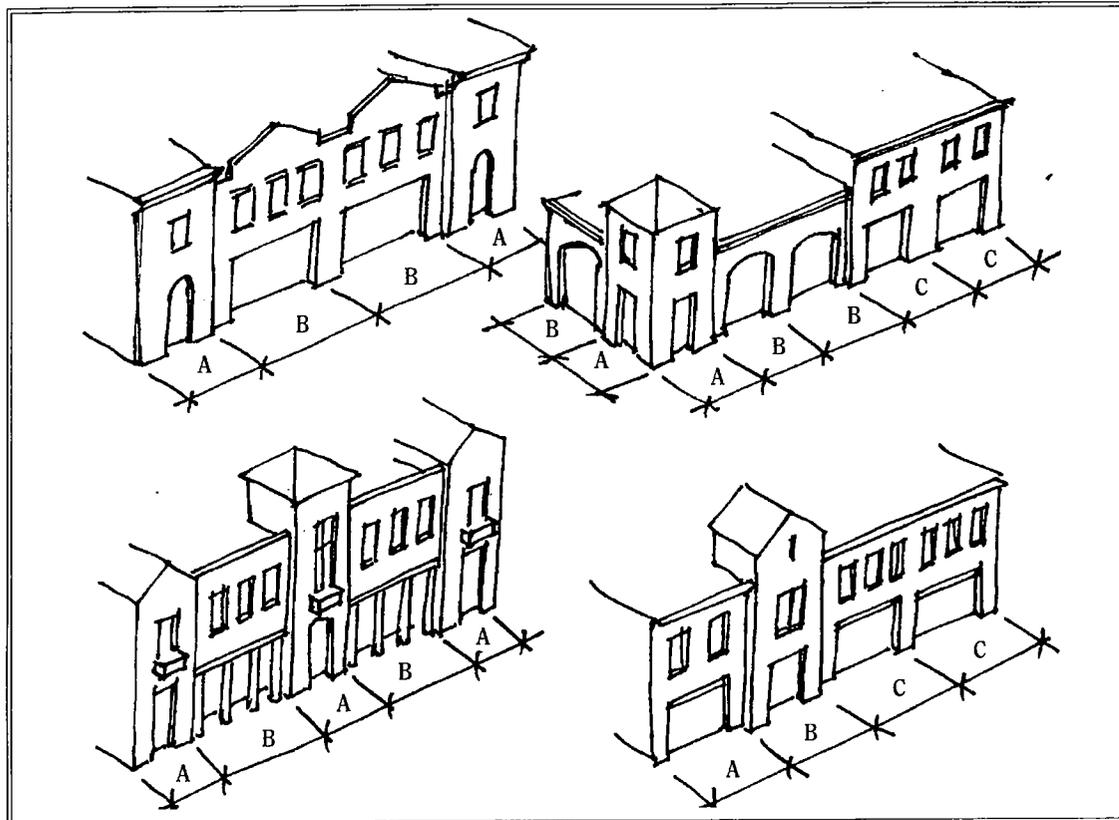
building part to another building part, and from each building to the next, which creates the larger streetscape and determines the dynamics of that streetscape - that is "how it feels".

A row of several blank building fronts with occasional doorways and few windows does not create a very dynamic and therefore interesting streetscape. There may be a "sense of place", but it is not a place we want to be. Likewise, a row of totally unrelated and random buildings with differing setbacks, unrelated window, door and opening types, varying and unrelated details, will result in a chaotic and uncomfortable streetscape, lacking continuity and a "sense of place".

Adjacent building facades need not share identical proportions or rhythms. Rather, each building design should first strive to be internally consistent. But if a number of adjacent



THE HISTORIC BUILDING INCREMENT IS EVIDENT IN THIS VIEW OF FILLMORE, 1993



BUILDING PROPORTION, RHYTHM AND PUNCTUATION

FIGURE 4.16

buildings exist with little or no design relationship, the streetscape will suffer for the lack of continuity. A strong underlying relationship of proportions and rhythms, with occasional points of “punctuation” (or design emphasis), will, more than any other factor, establish and enhance a clear “sense of place” in the CBD.

Refer to Figure 4.16 and 4.17 in reference to the following items:

A. PROPORTION AND RHYTHM

All buildings within the CBD should respond sympathetically to the established proportions and rhythms of existing buildings and development patterns within the CBD, particularly those of historic character. This can be achieved by addressing the following:

1. Each building facade design should establish a clear proportional hierarchy between its vari-

ous parts, consistent with the “Street Level Building Increment” and “Upper Level Building Increment” guidelines, above.

- a. The design of larger portions of building mass, such as extended or unbroken walls, sloped roof planes, etc., can be refined and balanced by grouping design components, such as openings, windows, columns and specific details. This design approach can change an otherwise large, unbroken plane into a set of related and interconnected “parts”.
2. In keeping with the character of the historic period, the ground floor of any building should establish a strong architectural base, with a floor to ceiling height ranging between twelve (12) and sixteen (16) feet.
3. Storefronts, windows, and other architectural features should be grouped to create pronounced rhythms in the building facade, as described above.
4. In a larger building, the repetition of certain parts may be quite extensive, but even in smaller buildings, a number of perceived rhythms may co-exist. A deliberate change in pattern at, for instance, an entryway or a building corner, creates a punctuation, a change in the visual rhythm not unlike the change in the rhythm of a musical score at the end of a stanza. However, these points of “punctuation” or emphasis should not occur too often, nor should every building facade necessarily in-

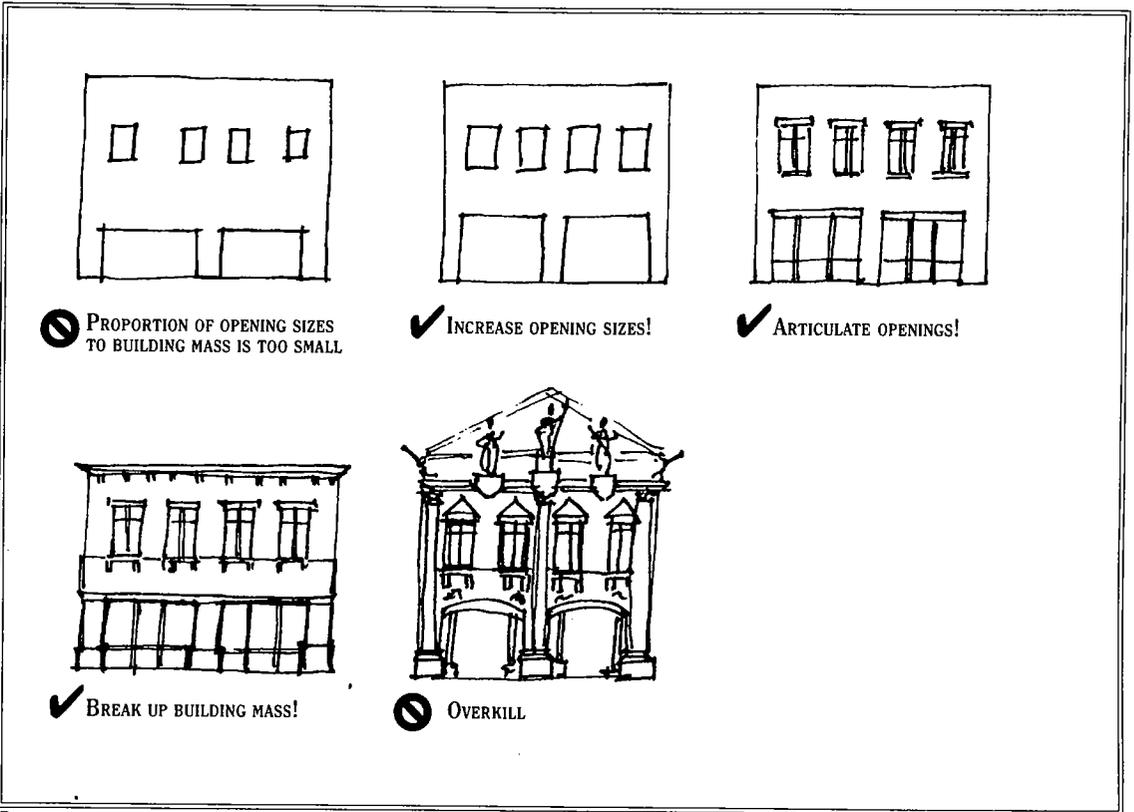
clude them, or they will lose their significance. Figures (4.16) and (4.17) illustrate some of the ways that building proportion, rhythm and punctuation can be manipulated to achieve an architectural response compatible with the character of the CBD.

B. ARCHITECTURAL ELEMENTS

Every building in the CBD should incorporate some combination of architectural elements common to the historic building fabric, several of which are identified below:

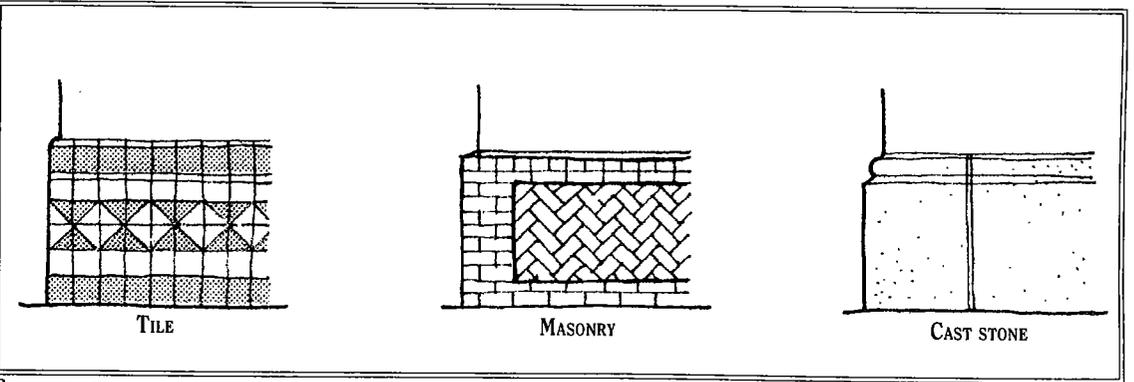
1. **Building Base** (fig. 4.18) - Where the building meets the sidewalk, and extending two (2) or more feet up the building face, is the opportunity to establish an architectural base. This base may be as simple as a change in surface texture, a projection or break in the wall plane, or a change in material or color. A substantial base may incorporate all of the above, and may extend quite high up the face of the building, sometimes incorporating the entire ground floor of a two or three story building.

- a. It is desirable to provide a base material that is highly resistant to damage, defacing and general wear and tear. Pre-cast decorative concrete, stone masonry, brick and commercial grade ceramic tile are examples of excellent base materials.
- b. Stucco may be used at the building base, but its use is discouraged due to suscepti-



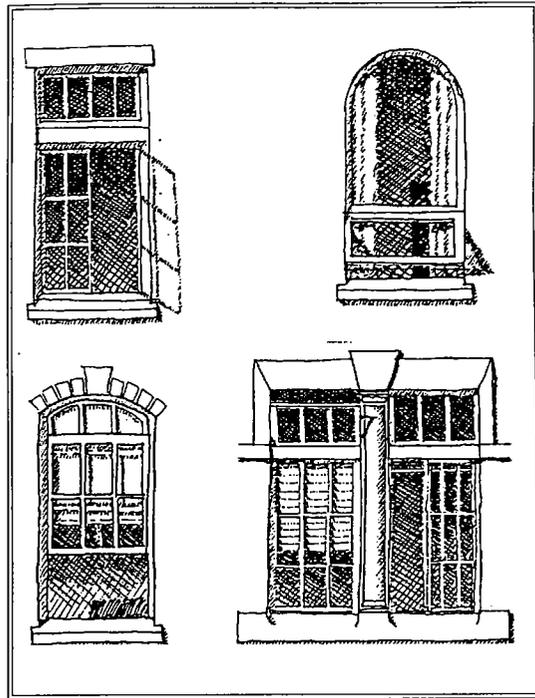
PROPORTION EXPRESSES THE HIERARCHY BETWEEN VARIOUS PARTS OF A BUILDING FACADE.

FIGURE 4.17



BUILDING BASE

FIGURE 4.18



WINDOWS ARE IMPORTANT DESIGN ELEMENTS

FIGURE 4.19

bility to cracking, staining and discoloration. Where stucco is used at the building base, it should be thoroughly sealed with a commercial grade sealer to enhance its resistance to such damage.

2. Windows, Doors and Openings - Windows, doors and other openings should be detailed to establish them as important parts of the total facade composition.

- a. In general, storefront openings should be horizontal, while upper level windows should be vertical or square in orientation.

Upper level windows may be paired to create a proportionate unit which is nearly square, or clustered to create pattern combinations which establish a rhythm of features of varying proportions which are related by the over-all facade composition

- c. Sills, headers and moldings can be used to frame and enhance the significance of an opening. In some cases an opening may warrant greater attention, such as at or above an entry. The molding and header may be enlarged or receive special detail in order to signify its importance in the over-all facade composition. Sills and headers can also be used to tie two adjacent windows together, or to unify actual windows and decorative wall panels (fig. 4.19).

3. Building Entries - In all cases entries accessible to the general public should be pronounced and easily recognizable. Along Central Avenue, Santa Clara and Main Streets, building entries should be placed a maximum of fifty feet apart.

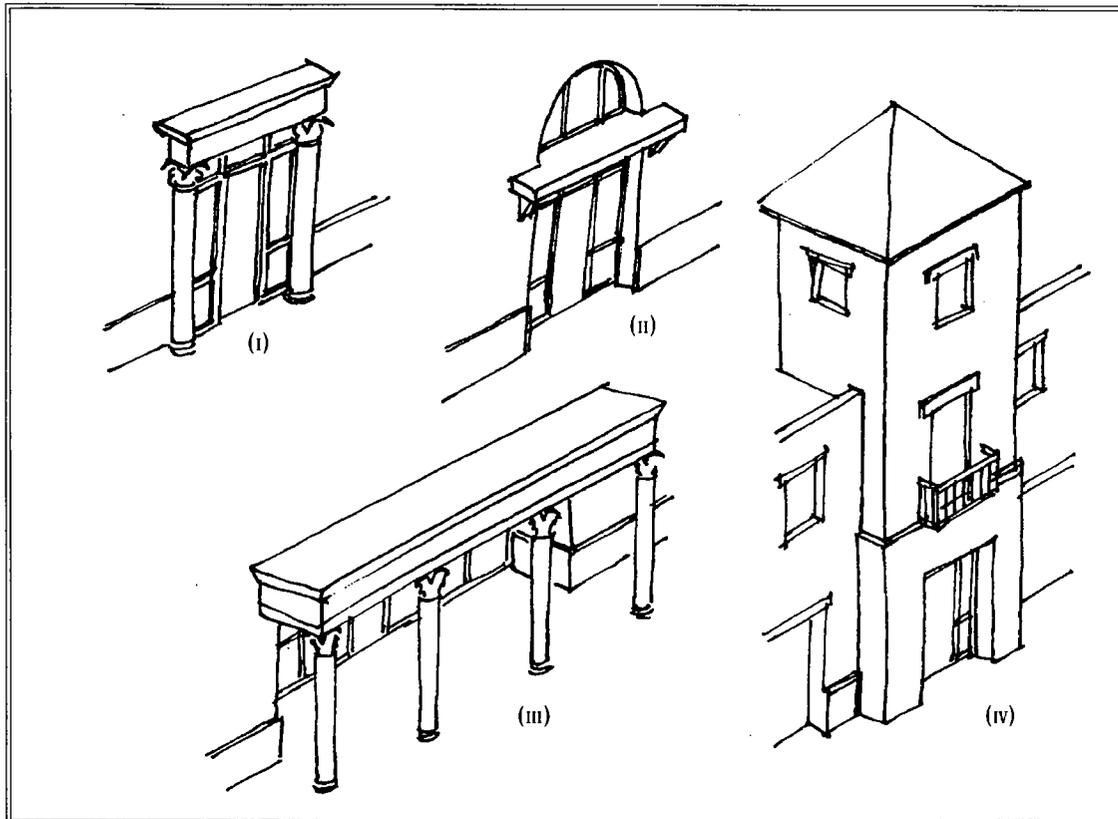
- a. **Main Entries** (other than storefront entries to street level shops):

The main entry to a building, leading to a lobby, stair or central corridor, should be emphasized at the street to announce a point of arrival, in one or more of the following ways (fig.4.20):

- i flanked by columns, decorative fixtures or other details;
 - ii recessed within a larger arched or cased decorative opening;
 - iii covered by means of a portico (formal porch) projecting from or set into the building face (refer to zoning guidelines for allowable projections);
 - iv punctuated by means of a change in roof line, a tower, or a break in the surface of the subject wall.
- b. **Corner Entries**
Buildings situated at the corner of a public street should provide a prominent corner entrance to street level shops or lobby space, in a manner consistent with Main Entries, as described above (refer to fig. 4.15 a,b,c and fig. 4.20).

c. **Side and Rear Entries**

Where such entries are intended to serve the general public, they should receive design treatment similar to that described in Item (a) above. Where such entries are intended *only as service or emergency access points*, they should be designed consistent with Item (d) below, except that any such entry which faces onto a public street or right-of-way should be enhanced, though to a lesser extent than a main entry, in a manner consistent with Item (a) above.



MAIN ENTRY SHOULD BE EMPHASIZED AT THE STREET TO ANNOUNCE A POINT OF ARRIVAL

FIGURE 4.20

d. Service Entries

Are those entries which are not intended for use by the general public. The service entry should be designed to blend into the surrounding building facade, provided with simple detailing, trim and finish consistent with the character of the building, such that it should not draw particular attention to itself, but should remain attractive and easy to identify.

- where trash and or storage areas are provided in conjunction with service entries, such areas should be completely enclosed and screened from public view, by the use of walls, doors and gates designed in a manner consistent with the architecture of the buildings.

4. Cornices and Parapets - Where flat roofs, or roofs which are not meant to be visible from

the street, are predominant in the building design, decorative cornices and parapet walls should be used to delineate the building profile, in a manner consistent with the historic character of the CBD (fig.4.11).

- a. Buildings with roofs as described above should utilize the following design elements:
- decorative and substantial parapet walls, incorporated to establish a distinct and interesting profile at the street elevation(s).
 - a substantial cornice should be used at the top of a parapet wall or roof





curb, providing a distinctive cap to the building facade. Lesser cornices of a similar architectural character may be used to complete the design at side or rear elevations which do not front on a public street.

5. *Pitched Roofs and Eaves* - In the context of the Fillmore CBD, as in most downtown environments, pitched roofs, visible from the street, are a rare exception to the predominant and relatively uniform stage set of flat roofs and decorative storefront parapets. Where pitched roofs are used, they are combined with ornamental eave detailing (i.e.: Fillmore State Bank) or are minimized, occurring only at the portion of the building fronting on the street, or at a focal element, such as a tower or portico.

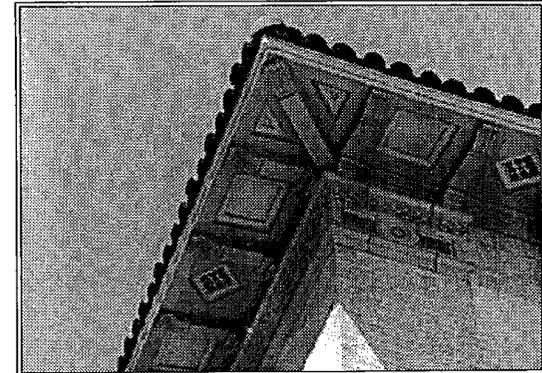
a. Pitched roofs should, in general, be reserved for prominently situated buildings. For example, those of a civic nature, or those occupying key sites (i.e.: street corners, or plaza/park frontage).

- eaves of pitched roofs should terminate with a decorative eave cornice complementary to the design of the building face.
- where overhang is provided, eaves should present either exposed rafter tails with decoratively shaped ends, or a decorated soffit with soffit vents incorporated into the design (fig. 4.22).

- plain and unadorned stucco eave soffit should be avoided.

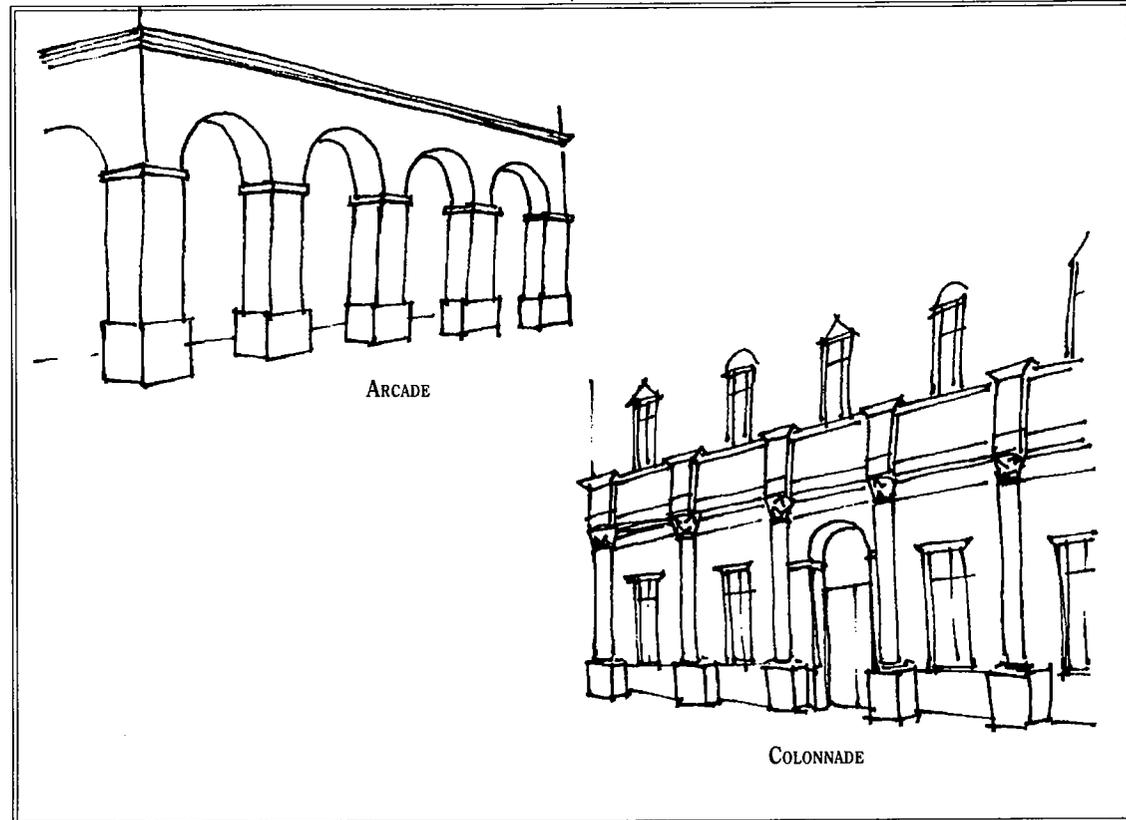
b. The slope of a pitched roof should generally not be less than 4:12. The roof plane of a lesser slope will likely not be visible from the street.

c. Rain gutters and downspouts should be concealed, unless designed as a specific architectural feature of the building. Where



ORNAMENTAL EAVE

FIGURE 4.21



ARCADES AND COLONNADES MAY BE INCORPORATED TO PROVIDE A RHYTHMIC REPETITION

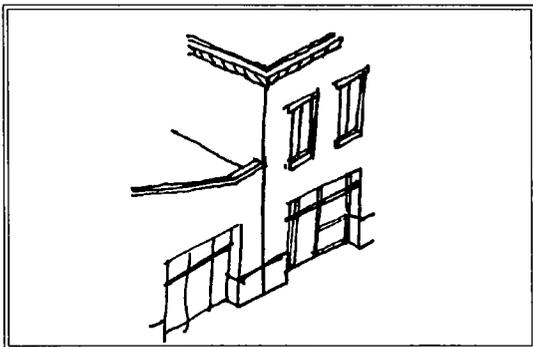
FIGURE 4.22

rain gutters are exposed as a design detail, they should be of copper, or properly primed and painted metal of a color complementary to the building design.

- downspouts should feed into a decorative scupper, which should return into a concealed downspout and drain, for conveyance of drainage under the sidewalk to the street or storm drain. Such drain systems are subject to review of the City Engineer.

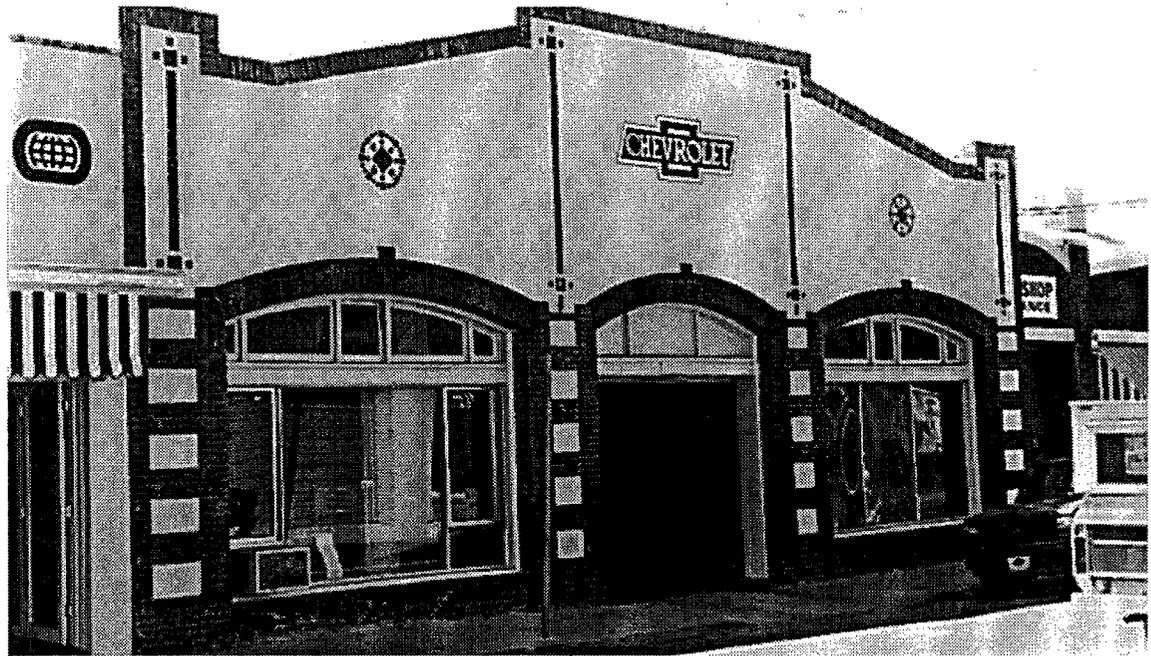
- d. Mansard “wrap around” roofs (“hats”) should be avoided. Partial mansards may be used where specifically related to the architectural style of the building.

6. *Arcades and Colonnades* (fig. 4.22) - Arcades and colonnades may be incorporated to provide a rhythmic repetition of column and storefront with ground floor uses set back behind a walkway, for display and access of ground floor commercial/ retail uses.



BLANK WALL AREAS

FIGURE 4.23



NEWLY RENOVATED FACADE WITH PERIOD SIGN.

- a. A “colonnaded building front”, which uses columns or column-like elements incorporated into the face of the building to establish a strong structural rhythm, may be used to accomplish an architectural effect similar to an arcade, without the provision of a covered walkway.
7. *Loggias* - Loggias, covered balconies or upper level terraces can be incorporated to provide an outdoor amenity while at the same time providing the visual building-edge required by the “build-to” line.
8. *Side and Rear Building Facades* - Building de-

sign character should be consistent over the entire building, especially where the building will be visible from publicly accessible areas, such as streets, alleys or parking areas. Although details may be simplified, elements such as cornices and window trim should be applied over the entire building.

- a. Corner buildings should be considered to have two front elevations, with the exception that the design may provide more emphasis to one elevation if there is a ‘main’ building entrance located there. All other details should be of equal emphasis.



9. *Blank Wall Areas* (fig. 4.23) - Walls without openings will not be permitted at the street front. Where a building abuts a side property line at the interior of a block, and where that building's side wall is likely to remain visible for an extended period of time, that building elevation should reflect a design treatment consistent with the building's established street-front 'Building Increment'.

3. STOREFRONT DESIGN

Interesting and enticing storefronts are perhaps the most crucial ingredient in promoting a vital and active street life in a commercial district. Storefronts should be generous, providing ample display window and entry points into shops, and a level of design detail which establishes some individuality for each shop or building, while assuring a relatedness from structure to structure. Refer to Figure 4.24 for the following discussion of criteria and design elements:

A. DESIGN CRITERIA

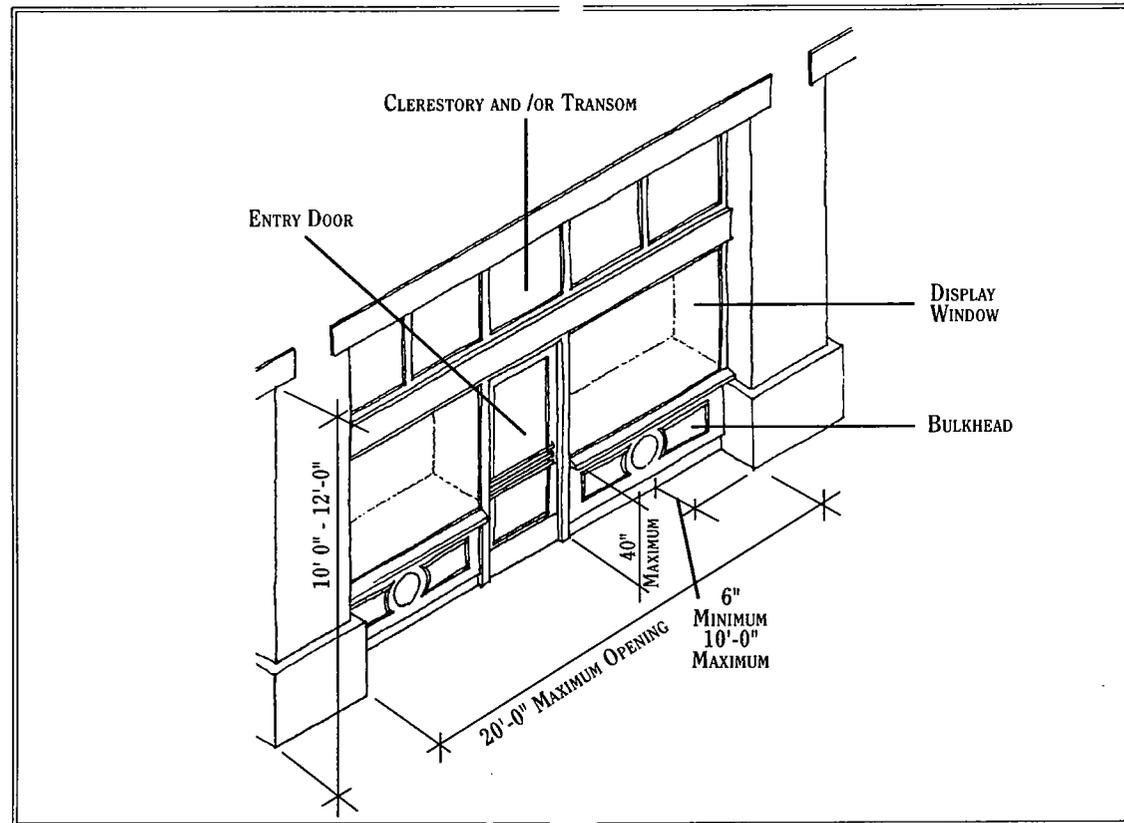
1. *Maximum Storefront Opening* - Storefront openings should not exceed twenty (20) feet in width without the interruption of a vertical building element, such as a column or wall.
2. *Storefront Height* - Storefront openings may extend up to fourteen (14) feet in height, but in general should range from ten (10) to twelve (12) feet in height.

3. *Recess* - Storefront window systems should be set into the building face. This will allow the building face material to return at the corner of the storefront opening, establishing a strong sense of mass and substance to the building.
 - a. Storefront windows should be recessed a minimum of six (6) inches from the building face, while eight (8) to twelve (12) inches is preferable.

- b. Storefront windows and entries may be recessed a maximum of ten (10) feet from the building face.

B. DESIGN ELEMENTS FOR STOREFRONT SYSTEMS

Traditional storefront systems are made up of several parts which, when integrated, provide the opportunity for endless variation in detail and character, while also establishing a relatedness from



STOREFRONT DESIGN CRITERIA

FIGURE 4.24



building to building. Storefront systems should address the inclusion of most or all of the following elements (fig. 4.25):

1. **Bulkhead** - At the base of the storefront, the bulkhead provides a transition between the sidewalk or paving and the glass display area.

a. Bulkhead may be of wood panel, tile, or masonry. Base materials should be compatible and consistent with the building design. Where wood is used, a curb detail should be provided to lift the wood slightly above the adjacent paved surface.

b. The Bulkhead and associated trim should not extend higher than forty (40) inches above adjacent paving, to assure ample window area above.

2. **Display Windows and Glazing** - These should be generous and highly transparent. Where a restaurant or other use requires visual privacy, multi-pane windows in combination with interior curtains may be utilized.

a. Windows should occupy sixty (60) to seventy (70) percent of the storefront system area. Multi-pane windows may be used, but are not encouraged for the majority of display window serving retail shop space.

b. Tinted windows are strongly discouraged, as they hinder pedestrian "window shop-

ping." Where sun control is a design factor, windows should be recessed and/or provided with awning or other shading device.

3. **Transom Windows** - These are upper glazed openings, above display windows, which are primarily intended to provide light to the interior of the shop space. These windows typically rest atop an intermediate beam or header which is exposed or trimmed-out with sill and glazing stops. Sometimes this beam is oversized to allow for shop signage to be placed integrally within the storefront system. Refer to Figure 4.49.

a. Transoms are recommended for street-level shop space in all commercial buildings within the CBD, and should be an integral part of the building's storefront system.



A CENTRAL AVENUE STOREFRONT





- b. Transoms need not be of transparent glass. There are a number of translucent glass types which will admit light while reducing heat gain and glare.

4. **Doors and Entry Treatment** - As previously stated, a building entry provides the opportunity to create a detailed point of arrival for pedestrian users. This portion of the building will receive more personal contact by users than any other on the building exterior. Thus, what is seen and touched should be of attractive and durable quality.

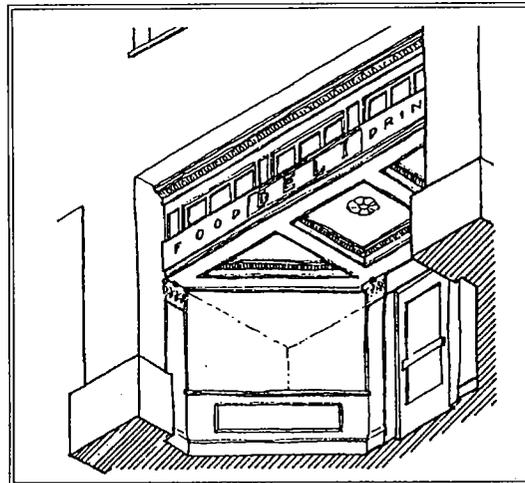
The following design elements should be considered to enhance building entries (figs. 4.25, 4.26, 4.27 a,b,c):

- a. Entry doors may be recessed to provide visual emphasis as well as protection from inclement weather;
- b. Where the entry is recessed, a decorative paving material, such as tile, marble or slate, may be used adjoining at the sidewalk edge;
- c. Doors should be decorative and substantial, with high quality and very durable hardware. Commercial grade materials should be used to accommodate long-term wear.
 - wooden doors should be provided with a 'kick plate' of brass or other durable material to resist wear at the door base.



DOOR AND ENTRY TREATMENT

FIGURE 4.25



ORNAMENTAL CEILING AT STOREFRONT ENTRY

FIGURE 4.26

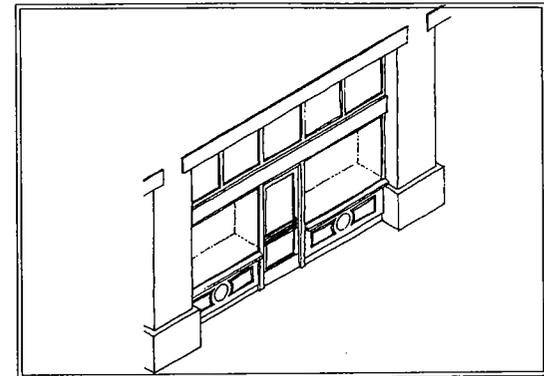


FIGURE 4.27A

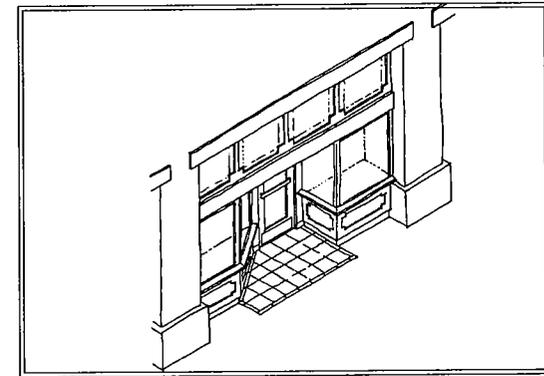
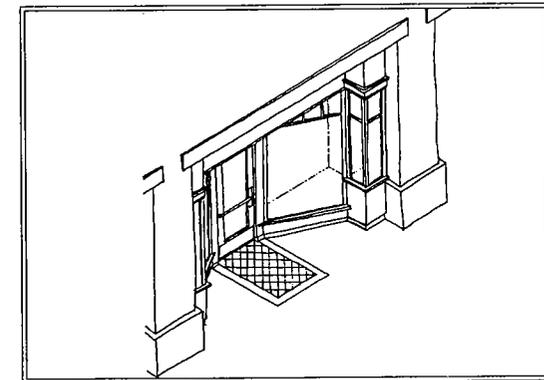


FIGURE 4.27B



STOREFRONT DOORS AND ENTRY TREATMENTS

FIGURE 4.27C



- d. Ornamental ceilings, with beamwork, molding or coffering should be incorporated (Fig. 4.26);
- e. Ornamental lighting should be incorporated, either wall mounted or suspended from an ornamental ceiling;
- f. A building directory or identifying plaque may be provided at the entry, per Section 9 of these guidelines.

5. *Recessed Entries In Storefront* - a common feature in traditional storefront design is the recessed display and entry area. This recess serves several valuable purposes: it provides weather protection at the entry, it expands the linear feet of window display that a shop may present to passers-by, and, along with awnings, it helps protect displayed merchandise from costly damage by direct and prolonged exposure to sunlight. See also Arcades and Colonnades (2.B.6).

- a. Maximum width of such recesses should not exceed sixty (60) percent of the storefront opening.
- b. Maximum depth of such recesses should not exceed ten (10) feet.

6. *Storefront Details and Materials* - In general, storefront systems should be of high quality and durable construction with traditional detailing, including moldings, beaded glass stops, paneled

soffits, sills, etc. Refer to items above regarding the design features of traditional storefront.

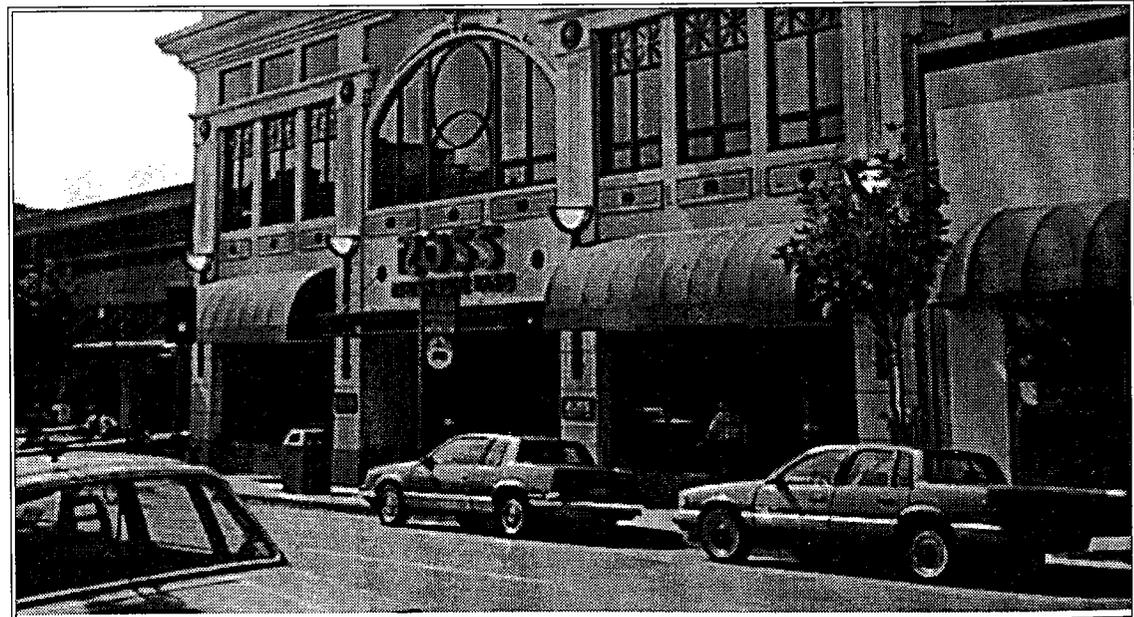
- a. Materials may include wood or metal, of a finished quality, typically painted.
- b. Unfinished aluminum storefront systems are not permitted.

7. *Renovated Storefronts* - Renovated storefronts should be restored to their original character where possible. Details and materials which match the original should be used throughout, especially where visible from the street. Where renovation is not feasible, the new storefront system should be designed in the spirit of the

original, incorporating like or similar details and configurations.

4. EXTERIOR BUILDING DESIGN

Materials should be selected based upon appropriateness to downtown Fillmore's early twentieth century character, the architectural style of the particular building, lasting beauty and durability of the finish. Although the accessibility of some traditional materials may be limited, many are still available and practical. There are also contemporary materials which are quite compatible with the desired character of downtown. See (Fig. 4.28).



THERE ARE CONTEMPORARY MATERIALS WHICH ARE QUITE COMPATIBLE WITH THE DESIRED CHARACTER OF DOWNTOWN.

FIGURE 4.28



Of primary importance is the use of quality products and the proper detailing and application of these products. One of the most important characteristics of buildings in downtown Fillmore is the obvious level of quality that went into the original work, both in terms of materials and details. Most downtown buildings have only two faces - a front and a back - to present to the public. The investment in quality can therefore be condensed and increased at these two faces, with an obvious desire to emphasize the street front.

A. EXTERIOR WALLS

Material for exterior walls will incorporate two aspects - color and texture. If the building's exterior design is complicated, with many "ins and outs", columns and design features, the wall texture should be simple and subdued. However, if the building design is simple (perhaps more monolithic), a finely textured material, such as patterned masonry, can greatly enrich the building's over-all character.

1. *Appropriate Materials* - The following materials are considered appropriate for buildings within the downtown. However, the number of different wall materials used on any one building should be kept to a minimum, ideally two or less:

- a. **Brick Masonry** - brick is a very versatile material available in a wide range of colors and sizes. Brick surfaces may be patterned by combining different colors, and coursework. Traditional brick work incor-



BRICK, SIMPLE RUNNING BOND

FIGURE 4.29



BRICK, WITH RELIEF DETAIL

FIGURE 4.31



BRICK, WITH VARIED COLOR BANDS

FIGURE 4.30

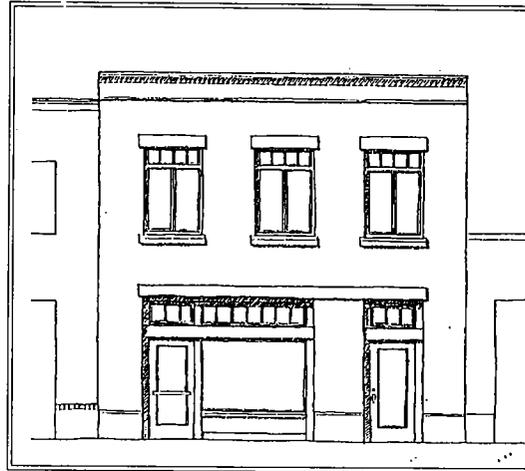
porates a wide array of detail applications expressive of structural and functional building components, such as: beams, headers, arches, sills, bases, trim, etc. See (figs. 4.29, 4.30, 4.31, 4.32).



BRICK IS VERSATILE AND RICH IN DETAIL AND TEXTURE.

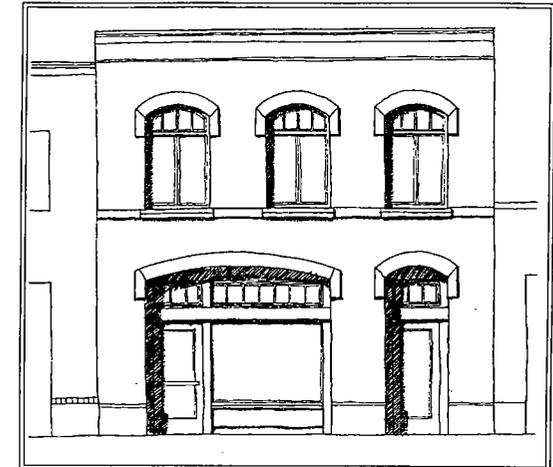
FIGURE 4.32

- application - in California, brick will almost always be used as a veneer or finish material over a frame of wood or steel, or as a facing for concrete masonry. Care should be taken to properly detail the veneer application in an authentic manner consistent with traditional masonry design. Edges and openings, such as at windows, should receive special care to assure that the masonry appears solid and substantial.
 - if “thin brick” or brick tile veneer is used, care should be taken to incorporate corner pieces and other details which serve to mask the veneer application.
 - the bonding pattern plays an essential role in the successful use of brick or other masonry. The stack bond should be avoided.
- b. Stone Masonry (and veneer)** - stone masonry, especially cut stone, is similar to brick in application and provides a very heavy and somewhat formal appearance. Stone also works well in combination with brick, providing a base with visual mass to “support” walls of brick masonry.
- stone may be used as an inset detail on brick facades for sills, headers or other accent features, such as keystones or wall caps.



STUCCO, WITH BASIC DETAIL EXPRESSED

FIGURE 4.33

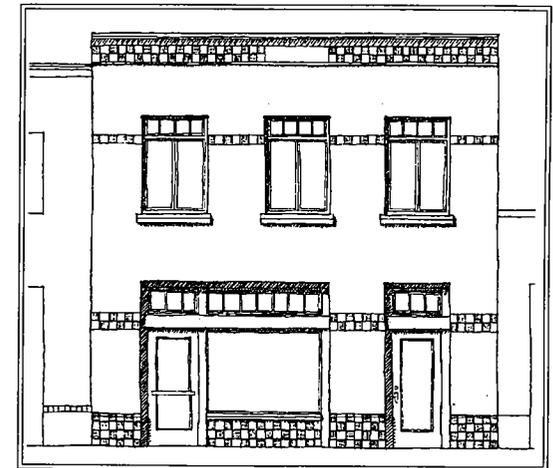


STUCCO, WITH MORE ELABORATE DETAILING

FIGURE 4.34

- c. Stucco** - stucco is a very versatile material. With proper detailing, a stucco building can achieve an elegance and level of refinement equal to that of brick masonry. In fact, the traditional “stucco” building was a masonry structure with an application of stucco applied as a finish material. For this reason, the stucco building should also convey a sense of mass and sturdiness similar to a brick or other masonry building. The same attention to detail applies here as with buildings of brick or stone. See (figs. 4.33, 4.34, 4.35).

- d. Concrete Block** - in certain applications, concrete block has proven to be a contemporary material worthy of incorporation into downtown architecture. The key to successful use of concrete block is in the detailing and finishing.



STUCCO WITH TILE

FIGURE 4.35

- e. Terra Cotta and other Glazed Tile** - Terra Cotta tile was a common finish material for urban buildings in the early part of this century. The bank building at the N.E. corner of Central Avenue and Main Street is a



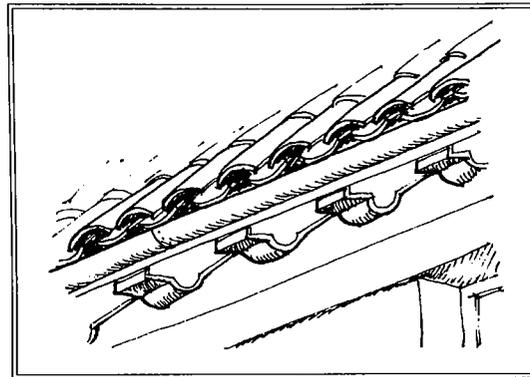
good example of the use of Terra Cotta. Other glazed ceramic materials, such as glazed tile, block or brick, may also be considered for use as an exterior wall finish. These types of materials have a hard, stain resistant surface which makes an excellent base material. See (fig. 4.35).

- only glazed materials graded for commercial use should be considered for use as an exterior finish material.

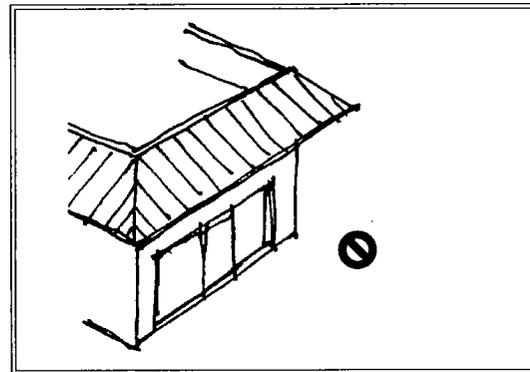
B. WINDOWS

Windows make up an important part of a building facade's composition. Windows are also the connector between the life and activity on the street and that within the building. Both of these "sensed experiences" are important in establishing an urban environment that is comfortable and friendly. Because of this, attention to window detail is important (fig. 4.19).

1. **Window ratio** - The amount of window, other than storefront systems, at street level should occupy between 50% and 60% of the wall area. Above the street level this ratio should fall to between 30% and 50%, depending upon building style and other architectural criteria.
2. **Materials** - Window sash and frame may be of wood or metal, painted or otherwise finished to provide a durable and long lasting surface. Where divided windows are desired, true-divided lites are strongly recommended. "Snap-



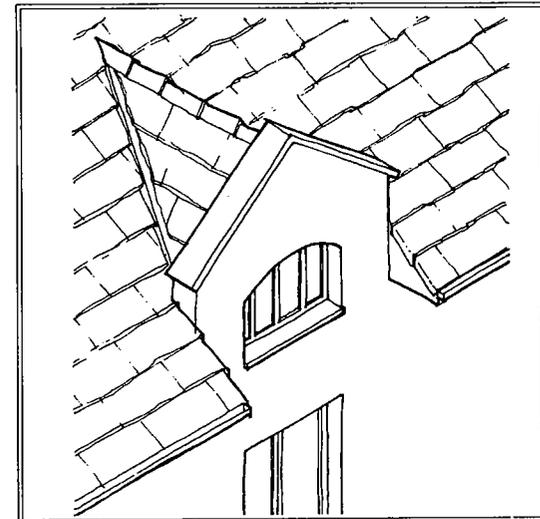
SLOPED ROOF TERMINATION WITH SHAPED RAFTER TAILS FIGURE 4.36



MANSARD TYPE ROOFS SHOULD BE AVOIDED FIGURE 4.37

in" type mullions or grilles which simulate this appearance are not recommended.

3. **Glazing** - Clear glazing is preferable, especially where street level display is the objective.
 - a. Where tinted glazing is used, the tint should be kept as light as possible. Dark windows create the impression of emptiness.
 - b. Reflective or mirrored glazing is not permitted.



DORMER TYPE ROOF ELEMENTS FIGURE 4.38

C. ROOFS

Visible roof elements should be designed consistent with the general building character. Materials and colors should compliment the style of the architecture.

1. **Sloped roofs** - should be designed to crown or punctuate the building. Roof elements should terminate with detailed eaves, utilizing eave molding, shaped rafter tails or other means of termination appropriate to the architecture of the building (fig. 4.36).
 - a. Mansard type roofs should be avoided, unless specifically related to the architectural style of the building (as in the "Victorian" style) (fig. 4.37).

- b. Dormer type roof elements may be incorporated into sloped roof designs, especially where such roof area occupies a substantial percentage of the buildings linear frontage (fig. 4.38).

2. *Acceptable materials include:*

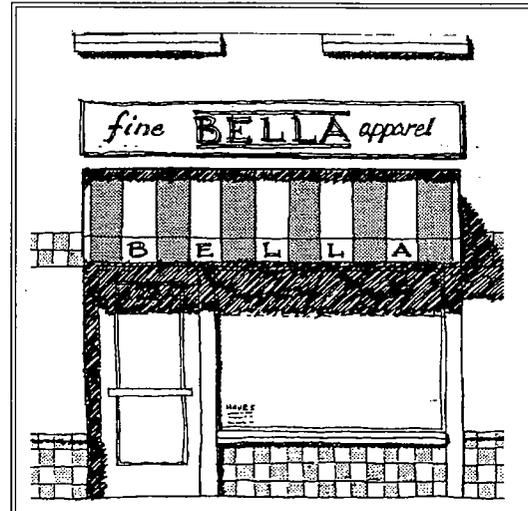
- a. Clay or concrete tile: commercial grade, of a color and style complimentary to the building design.
- b. Ceramic tile: on decorative elements such as domes or parapets.
- c. Metal seam: standing seam or batten seam, natural copper, anodized or factory coated finishes. Painted metal roofing is not recommended.

3. "Flat roof" - areas should be detailed and of a material to provide a clean, uniform appearance from windows of adjacent buildings.

D. BUILDING ACCESSORIES AND DETAILS

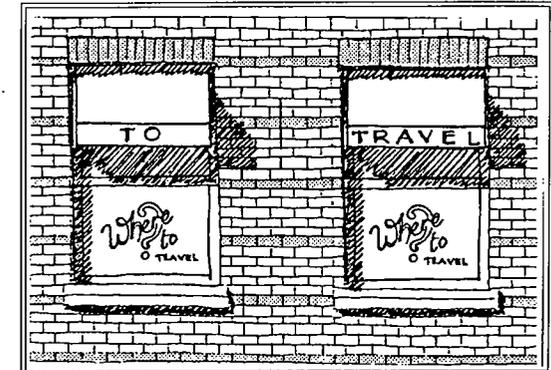
There are a number of design elements which may be incorporated into the building design, especially at street level, in order to add to the experience of the pedestrian while meeting important functional needs as well. The following accessories and details are recommended for inclusion in the building:

1. *Awnings* - awnings provide the opportunity for color and visual relief. Awnings serve a very



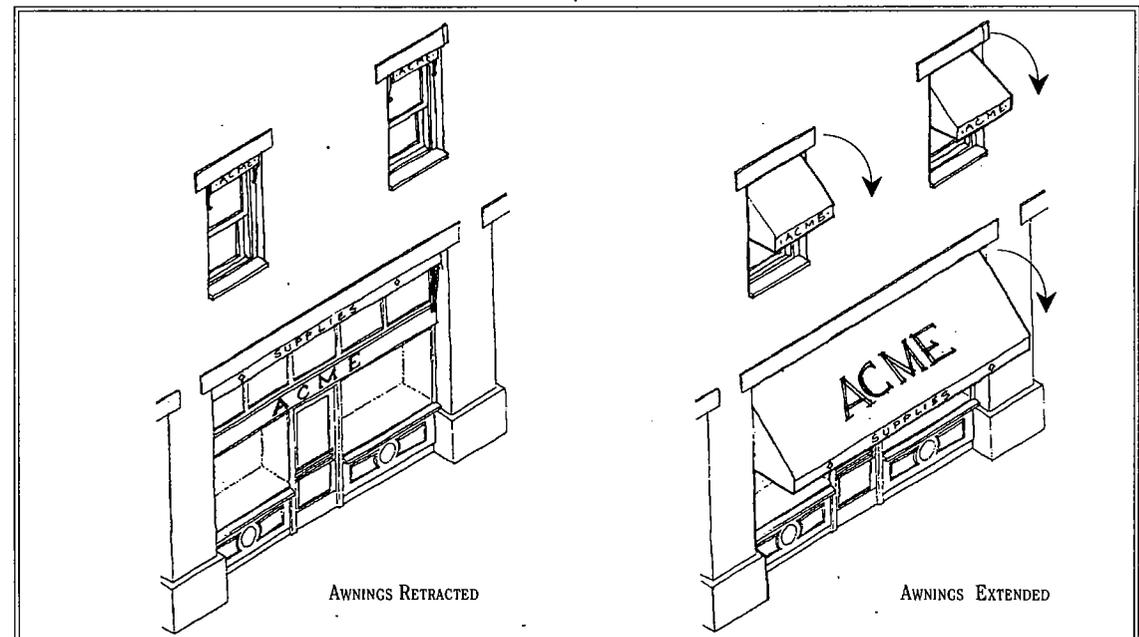
AWNINGS IN STOREFRONT

FIGURE 4.39



AWNINGS IN WINDOWS

FIGURE 4.40



AWNINGS RETRACTED

AWNINGS EXTENDED

RETRACTABLE AWNINGS SAVE ENERGY AND PROTECT MERCHANDISE

FIGURE 4.41

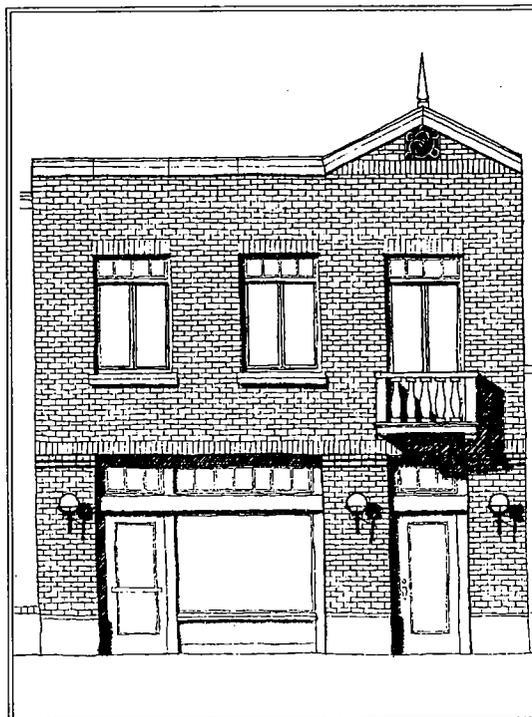
functional purpose by protecting shop windows from intense direct sunlight. The most functional awnings are “retractable”, having a frame and support structure with the ability to be adjusted up or down depending upon lighting conditions. Where awnings are used, it is recommended that they be of retractable design. See (figs. 4.39, 4.40, 4.41).

- a. Awnings should be of a durable, commercial grade fabric, canvas or similar material having a matte finish. Awning frames and supports should be of painted or coated metal or other non-corroding material.

- glossy or shiny plastic or similar awning material is not recommended.

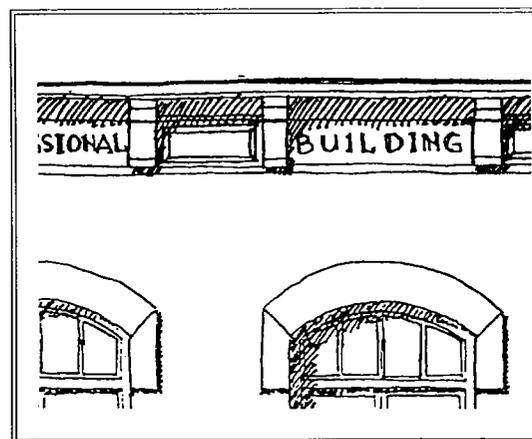
2. *Grillework/Metalwork and Other Details* - there are a number of details, often thought of as mundane, which may be incorporated into the design to add a degree of visual richness and interest while meeting functional needs. Such details include items like the following:

- a. light fixtures, wall mounted or hung with decorative metal brackets (fig. 4.46a,b);
- b. metal grillework, at vent openings or as decorative features at windows, doorways or gates;
- c. decorative scuppers, catches and downspouts, preferably of copper;



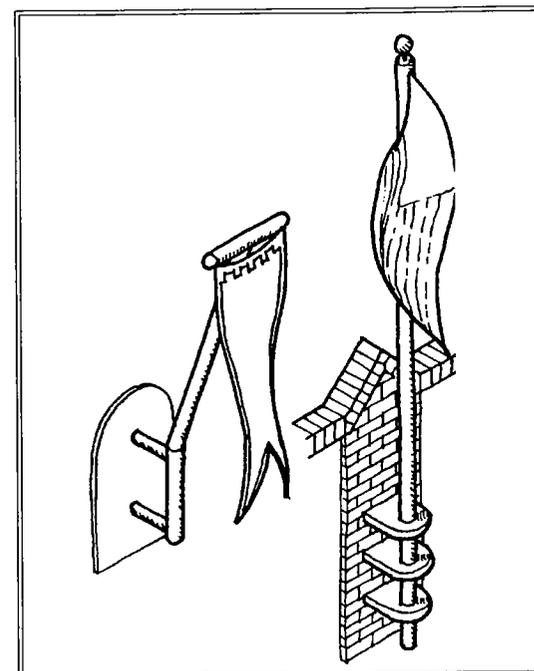
BALCONIES, RAILS AND FINIALS

FIGURE 4.42



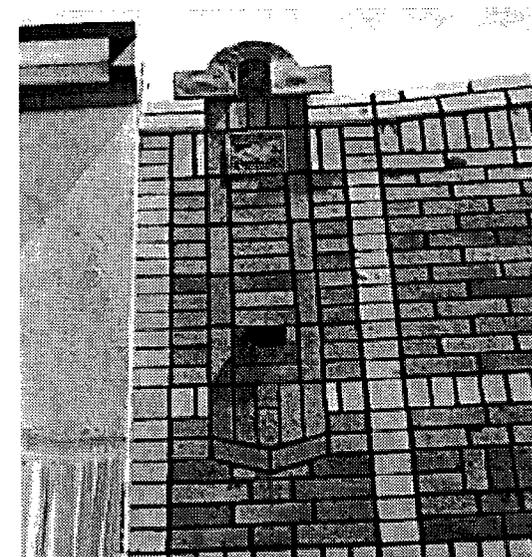
CORBELS

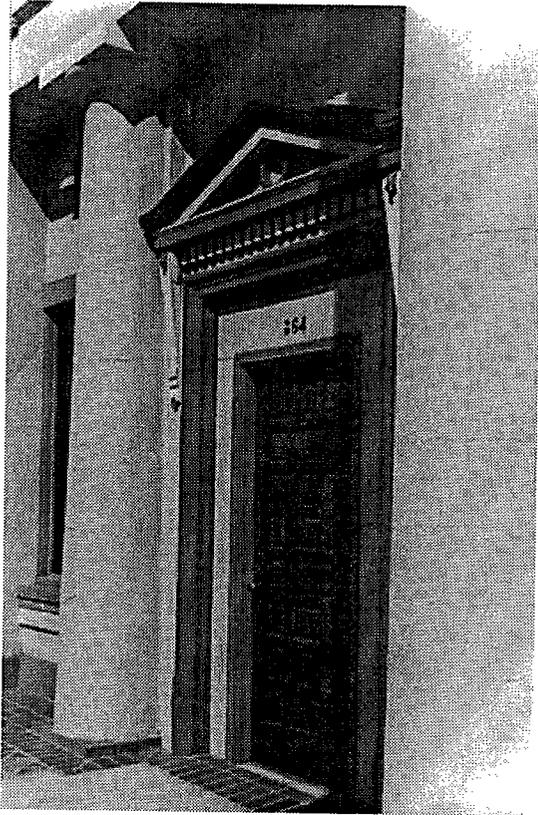
FIGURE 4.43



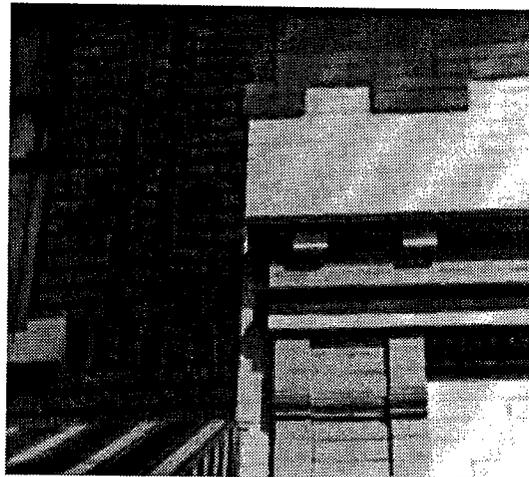
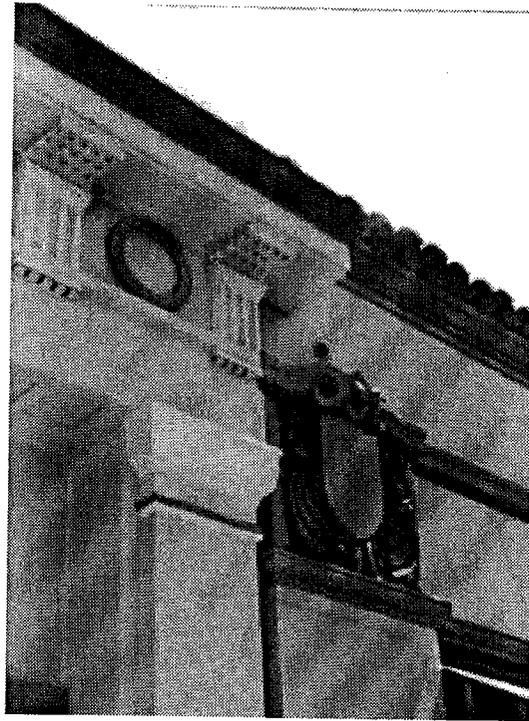
FLAG OR BANNER POLE BRACKETS

FIGURE 4.44





- d. balconies, rails, finials, corbels (fig. 4.42, 4.43), plaques, etc.;
- e. flag or banner pole brackets (fig. 4.44);
- f. fire sprinkler stand pipe enclosures and hose-bib covers, preferably of brass.



E. EXTERIOR COLOR DESIGN

Exterior colors play an important role in the way we perceive a building and its details. Colors can be used to draw attention to specific parts of a building, such as entries. Color can also be used to mask or diminish the visual importance of a particular feature, for instance a service area, simply by altering colors, or the contrast between surfaces and details, in a certain area. In general, contrast (light against dark or dark against light) will call for your attention, for better or for worse. Consideration for these issues should be giving as noted below:

1. *Color Design Exhibit* - each building design submittal within the CBD should include a "color design" exhibit identifying the exterior colors and materials proposed for use.
2. *Primary Exterior Building Colors* - should be muted, while trim and detail colors should provide a contrasting accent.
 - neutral or light colored walls should be contrasted with a darker more intense trim color, while dark colored walls should be contrasted with light colored accents and details.
3. *Finish Materials with Natural Colors* - materials such as brick, stone, copper, etc. should be used where practicable.
4. *Bright or Intense Colors* - should be used sparingly, and should typically be reserved for more refined or delicate detailing, such as grillework, as well as more transient features such as awnings, signs and banners.



5. ADDITIONS, RENOVATIONS AND RESTORATIONS

With few exceptions, buildings within the CBD already possess the desired characteristics which this Plan is intended to support. Modifications to buildings with these characteristics, and especially to buildings having historical significance, must be done with respect for this existing character, and in accordance with the following:

A. PRESERVATION OF TRADITIONAL FEATURES AND DETAILS

Sensitive response to existing materials, details, proportions, as well as patterns of materials and openings is required when any such work will affect the appearance of an existing building's exterior.

1. *Windows and Window Replacement* - where existing windows are to be replaced, they should match the originals in design character. Where new window openings are to be provided, sizes and patterns of existing window openings should be matched or closely replicated.
 - in the case of historically designated buildings, replacement of existing windows should be done in such a manner as to match the originals as closely as possible.

2. *Doors and Door Replacement* - where existing doors are to be replaced, they should match the originals in design character. Where new door openings are to be provided, sizes and patterns of existing door openings should be matched or closely replicated.

- in the case of historically designated buildings, replacement of existing doors should be done in such a manner as to match the originals as closely as possible.

B. REMOVAL OF ELEMENTS INCONSISTENT WITH ORIGINAL FACADE

Buildings are often altered over time in an effort by owners or shop keepers to 'keep up with changing times' or to 'remake a tired image'. Unfortunately, such changes are often done in a 'tack-on' manner and result in gradual but severe erosion of the original character and cohesion of the downtown. Restoration of buildings which have been substantially or carelessly altered is strongly encouraged.

1. Where an existing building is to be substantially modified (as per the CBD zone development standards), there is an opportunity to undo work which may have been constructed over time which neglects the detail and character of the original building. Where feasible, this work should be removed or otherwise altered to correct design inconsistencies and to reestablish the over-all design integrity of the original structure.

C. ADDITIONS TO EXISTING BUILDINGS

Additions should be done with care and sensitivity to assure a compatibility between the old and new. In general, any addition to an existing building within the CBD should reflect one of the following two basic approaches:

1. *Replication of Existing Character* - in which the basic design approach takes on the proportions, details and features of the existing building in a manner very similar or identical to the original construction.



AN EXISTING BUILDING, BEFORE RENOVATION



AFTER RENOVATION



2. *Interpretation of Existing Character* - in which the new design responds to the existing building 'sympathetically'. For example, using like proportions in combination with stylized or simplified details inspired by the originals.

D. SEISMIC RETROFITTING

Where structural improvements for seismic retrofitting are to affect the building exterior, such improvements should be done with care and consideration for the impact on appearance of the building. Where possible, such work should be concealed. Where this is not possible or practicable, the improvements should be planned to carefully integrate into the existing building design.

Seismic tie straps, to secure floor and roof framing to the adjacent walls, are one of the most common improvements made today. However, the straps are often installed by workers with no guidance about the careful placement of the ties. Often the tie holes are drilled without alignment or through key features or details, resulting in a haphazard and destructive result which can permanently scar the building.

1. Seismic improvements should receive the same care and forethought as any other building modification, and an exterior building elevation is required showing the location and appearance of all such improvements.





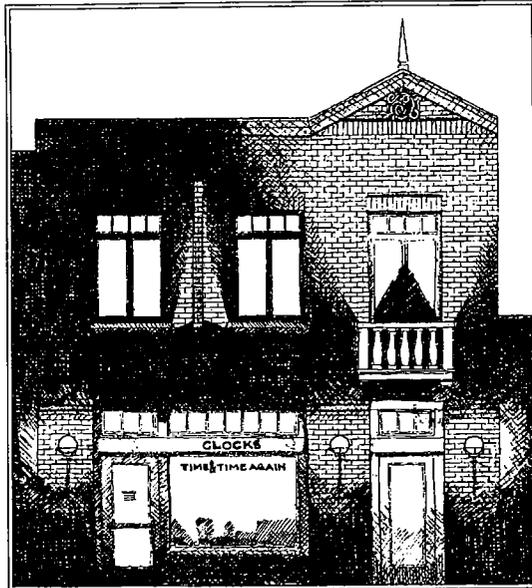
6. ARCHITECTURAL LIGHTING

Lighting should be considered as an integral part of building design. Proper lighting design will have a positive effect on the building and its users, as well as help to promote a safe and enjoyable nighttime pedestrian environment in the downtown core.

A. TYPES OF LIGHTING

Within the Specific Plan project area there will be a need for several exterior lighting types:

1. *Area Lighting* - for public and private parking lots, alley ways, parkways and walks. Such lighting should be set in a manner that assures maximum lighting benefit without allowing stray light to intrude into windows of nearby residents or to create glare problems for nearby automobile traffic.
2. *Ornamental Lighting* - not intended to light large areas or wall surfaces. Ornamental lighting plays an important role in bringing visual life to streets at night. Colors, finishes and design of ornamental lighting should be compatible with the design characteristics of the architecture (figs. 4.45, 4.46a and b).
3. *'Hidden Source' Lighting* - for certain prominent architectural features, hidden source lighting can be used to create dramatic effects, illuminating towers or other unique architectural features. Such lighting can be concealed



ORNAMENTAL LIGHTING BRINGS STREETS TO LIFE AT NIGHT FIGURE 4.45

in soffits, behind ledges or parapets, or even set into landscape areas with the light directed at the desired element to be highlighted.

B. DESIGN INTEGRATION

It is recommended that a lighting design professional be consulted as an integral part of the design process for determining types and intensities of lighting to be used. In addition, the following 'rules of thumb' should apply:

1. Use the minimum brightness necessary for illumination of large areas.
2. Use brighter, more intense lighting to highlight architectural features and focal points.



FIGURE 4.46a

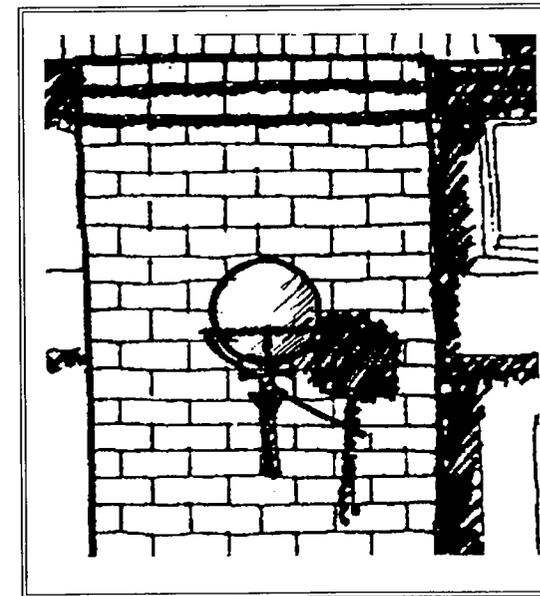


FIGURE 4.46b



C. MATERIALS

Lighting fixture materials should be compatible with the design objectives of this Specific Plan.

1. Where lighting is mounted to a building, its materials, finishes and colors should compliment and coordinate with those of the building.
2. Recommended Lighting Glass and Globes include:
 - a. clear acrylic or poly carbonate with fresnal patterns;
 - b. borosilicate glass;
 - c. translucent clear-frosted or white acrylic or polycarbonate globes.
3. Glass and Globes which are *not* recommended:
 - a. clear or tinted smooth finish acrylic or polycarbonate globes;
 - b. any dark-tinted or mirrored glass diffusers
4. Recommended lamp types include:
 - a. color corrected high pressure sodium (HPS);
 - b. color corrected metal halide (3,000 degrees K);
 - c. color corrected standard fluorescent (2,700 -3,000 degrees K).
5. Lamps which are *not* recommended include:
 - a. Standard "peach" high pressure sodium;
 - b. low pressure sodium;
 - c. standard mercury vapor;
 - d. non-color-corrected, standard fluorescent.

It is recommended that a professional lighting consultant be involved in projects where business success depends upon quality lighting and display.



CENTRAL BUSINESS DISTRICT CORE AREA SIGN GUIDELINES

Commercial signs are one of the most important elements of a healthy downtown core. Signage communicates more than just the name of a business. It communicates the degree to which the businesses in that core are alive and whether or not those businesses still care about the business they are doing. A well maintained sign, with bright colors and lighting that is functional, says to potential customers “We are here, we care and we want your business”. Likewise, a dull and worn sign says “We are tired and may not be around much longer - your business isn’t that important to us.” Unfortunately, many downtowns suffer from too many of the latter and not enough of the former.

These guidelines are intended to encourage excellence in signage, as a communication tool and as an art form, to attract customers, make a statement about the quality of the businesses represented, and to contribute positively to the over-all quality of the downtown core. Businesses are encouraged to use signage in a manner that will clearly express the spirit of their unique enterprises while at the same time enriching the visual life of the downtown street for everyone.

Note: Refer to City of Fillmore’s CBD Zoning Standards for specific requirements and allowable sign area.

1. TYPES OF PERMITTED SIGNS

The following sign types are permitted within the CBD (refer to figs. 4.47, 4.48, 4.49 and 4.50):

A. WALL SIGNS

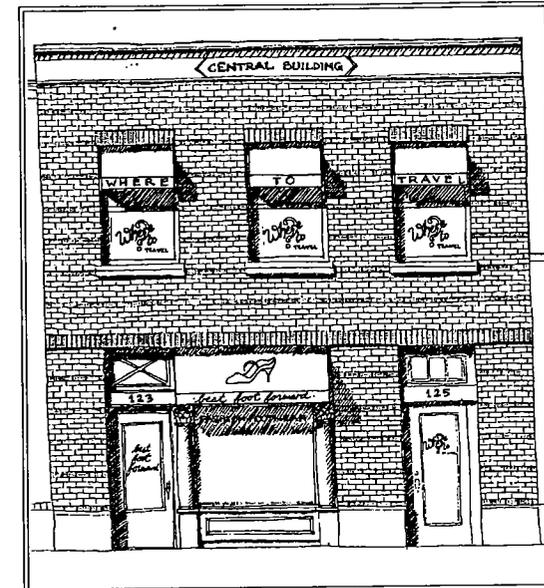
Wall signs should be located on the building face in an area specifically intended for signage, or where such signage will appear integral to the building design. Such signs may be as follows:

1. Flush-mounted - attached panel or individually mounted letters.
 - such signs should not block or cover any significant architectural features or details of the building.
2. Painted - applied directly onto the building face in an appropriate location.
3. “Can” or cabinet type wall signs are not permitted.

B. PROJECTING SIGNS, MARQUEES

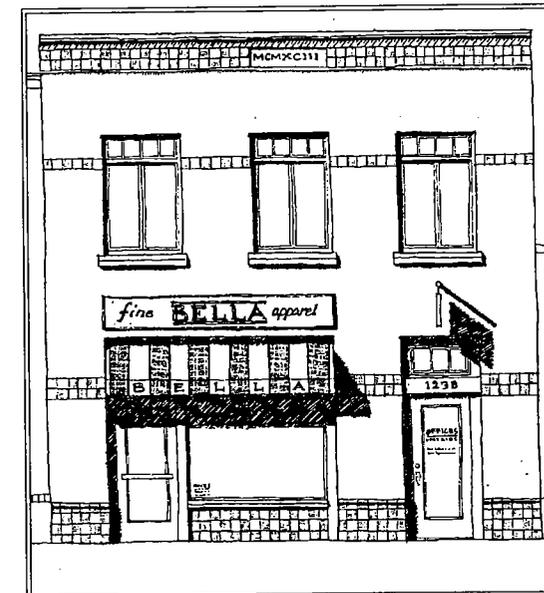
Projecting signs and marquees may be attached to the building such that no portion of the sign extends lower than seven (7) feet above adjacent finished grade, nor may any portion extend beyond three (3) feet from face of building wall.

1. Projecting signs may be directly attached to the building face, or suspended from decorative brackets, as illustrated in Figure 4.52.



TYPICAL PERMITTED SIGNS

FIGURE 4.47



TYPICAL PERMITTED SIGNS

FIGURE 4.48

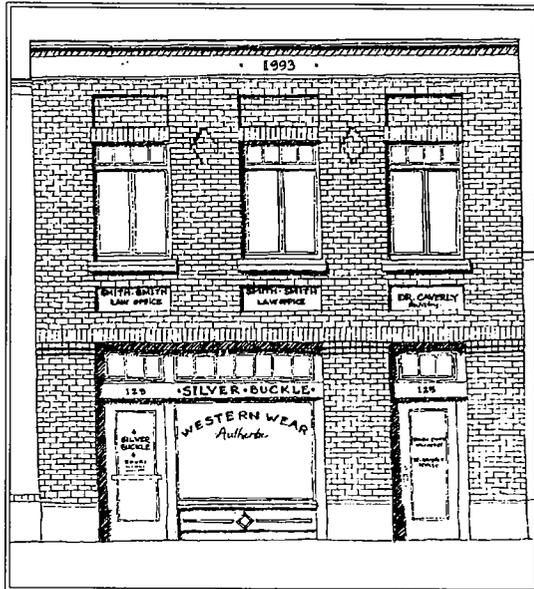


FIGURE 4.49



FIGURE 4.50

- The sign face and structural support should be designed as a compatible unit and should be attached to the building in a manner which will not damage or interfere with the building character.

C. FIGURATIVE SIGNS

Signs which advertise the occupant business through the use of graphic or crafted symbols, such as shoes, keys, glasses, books, etc. are encouraged. Figurative signs may be incorporated into any of the allowable sign types identified here (refer to Fig. 4.51).

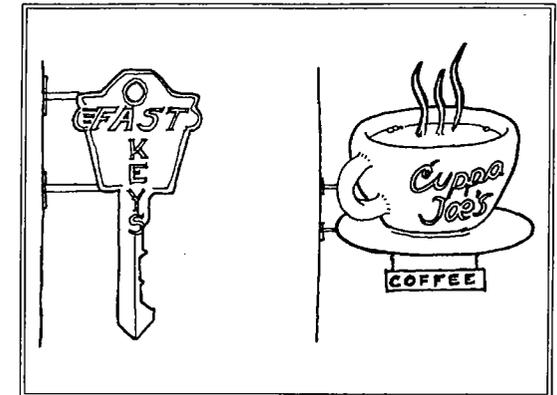
D. AWNING AND CANOPY SIGNS

Such signs should be integrated into the design of the awning or canopy, subject to the following:

- Sign area should be limited to fifty (50) percent of the total awning or canopy area.
- Internal or "back-lit" illumination is not recommended.

E. APPLIED WINDOW SIGNS

Window signs must be applied or painted directly on to the glass, and should not exceed fifty (50) percent of the window area at street level, or twenty-five (25) percent of the window at the second floor.



FIGURATIVE SIGNS

FIGURE 4.51

- Window signs may not be painted over an opaque background, in order to preserve transparency of the window.

F. DIRECTIONAL, ALLEY AND SERVICE SIGNS

Such sign should be discreet and integrated into the design of the building being served.

G. MURALS

Murals will be allowed only on blank side walls of buildings, and are subject to review and approval of the Planning Director.



BRACKET MOUNTED PROJECTING SIGN

FIGURE 4.52



2. DESIGN INTEGRATION

In all cases, the location, style and detail of the sign should be compatible with, and reflect the spirit and quality of, the City of Fillmore's objectives for the Downtown Core, utilizing text, graphics and design features which promote the image of "small town America's Main Street."

A. SIGN LIGHTING

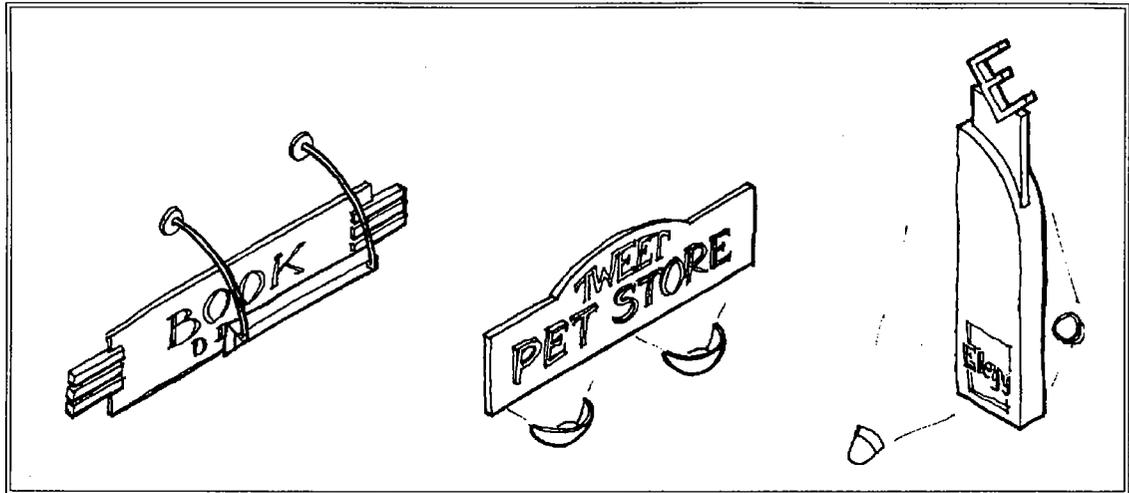
Lighting should be considered as an integral part of sign design, and may be either internal or external, with the following considerations:

1. Where light fixtures are visible, they should be compatible in character with both the sign and the building design.
2. Outside lighting must be directional, and should be positioned such that no glare or hazard is created for onlookers.

B. RECOMMENDED MATERIALS

The following materials are recommended for use in signs displayed in the CBD:

1. Wood - carved, sandblasted, etched, and properly sealed, primed and painted, or stained.
2. Metal - formed, etched, cast, engraved, and properly primed and painted or factory coated to protect against corrosion.



LIGHTING SHOULD BE CONSIDERED AS AN INTEGRAL PART OF SIGN DESIGN

FIGURE 4.53

3. High density pre-formed foam or similar material - new materials may be very appropriate if properly designed in a manner consistent with these guidelines, and painted or otherwise finished to compliment the architecture.
4. Custom neon tubing, in the form of graphics or lettering, may be incorporated into several of the above permitted sign types.
5. Unique materials and designs which are not identified here, but which are compatible with the objectives of this Specific Plan may be considered, subject to review and approval by the Planning Director.



QUALITY SIGNS ARE AN INTEGRAL PART OF A LIVELY STREET.

ADDENDUM

Per City of Fillmore Ordinance 98-727 adopted March 10, 1998, Chapter 4 of the Downtown Specific Plan, Section: Private Property Design Element has been revised to include Central Business City Core Area Sign Guidelines paragraph 1H and Railroad Property Sign Guidelines paragraph I.

Portable Sidewalk Signs

Portable signs shall be no larger than twenty-four (24) inches wide by four (4) feet in height and shall be installed and removed from the sidewalk daily by the business owner.

1. Signs shall be designed as Figurative Signs.
2. A-Frame signs are not permitted. But, A-Frame signs are permitted on a temporary basis at 15-day periods for non-businesses to announce special events or noticing.

B

CBD TRANSITIONAL AREA

CHARACTER AND SCALE OF THE CBD TRANSITIONAL AREA

The Central Business District Transitional Area is intended to serve as a physical transition between the more intense building types of the CBD Core Area and the less intense surrounding residential neighborhoods. While the permitted uses in this area will be largely the same as those of the CBD Core Area, the architecture is intended to reflect the development character of a high quality turn-of-the-century neighborhood. It is not uncommon to find older buildings in the vicinity of a downtown core being adapted to include commercial uses, such as professional offices, specialty shops, bed and breakfasts, etc. It is this quality and character that the CBD Transitional Area seeks to encourage.

Within the CBD Transitional Area, the architecture should be compatible with that of the adjacent neighborhoods. The following architectural styles are appropriate sources of inspiration for new development:

- Craftsmen
- Victorian

- Queen Anne
- Shingle-style

CBD TRANSITIONAL AREA BUILDING DESIGN GUIDELINES

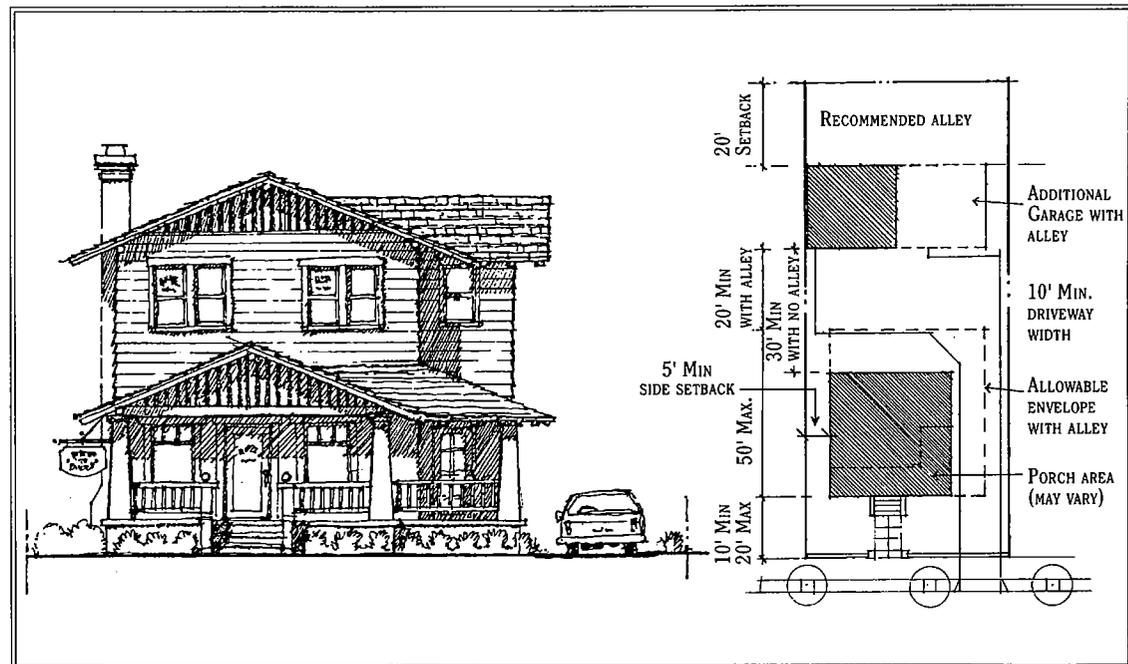
These guidelines identify some key design elements which should be considered in the project design to assure compatibility with the City's overall Design Objectives for this area.

BUILDING DESIGN CRITERIA

1. Building Mass and Organization - buildings should be designed to be compatible with the

adjacent and nearby neighborhoods to the north and south, utilizing, or incorporating strong reference to, traditional residential scale, detail and materials, as identified below.

2. This plan identifies three basic building types appropriate for use within the CBD Transitional Area:
 - The "House" type - characterized by distinguished roof forms, broad eaves, a generous porch, pronounced entryways and feature windows.
 - The "Village Mixed-use" type - incorporating traditional storefront shop space (design per CBD Core Area design guidelines) with street front pedestrian access to residential units above.



"HOUSE TYPE" (OPTIONAL ALLEY ACCESS)

50 FT. LOT WIDTH



The "Townhouse" type - a traditional townhouse design, incorporating front entry stoops to first floor living space raised slightly (2 to 3 feet) above street level.

- Each building should incorporate a front porch or stoop, which may be covered and partially enclosed by a decorative rail.
- Front building setback should be between ten and twenty feet from back of sidewalk. The resulting front yard should be defined at the back of sidewalk by means of decorative low wall of stone or masonry, wooden fence or hedge, and may be provided with a gate and/or entry arbor. Side set-backs for the main building should not be less than five feet. Garages may abut the property line at one side.

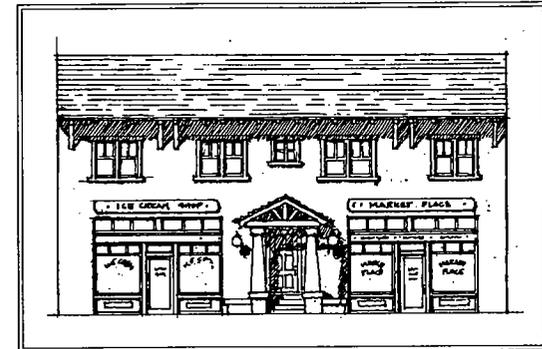
- Traditional elements, such as columns, exposed eaves, trellises, decorative chimneys and dormer windows should be incorporated into the design.

- Acceptable materials include:
Exterior walls: wood siding, stucco, brick masonry, stone.

Roofs: fiberglass composition roof shingles (higher quality, "dimensional" material only), concrete tile, clay tile, slate or similar material.

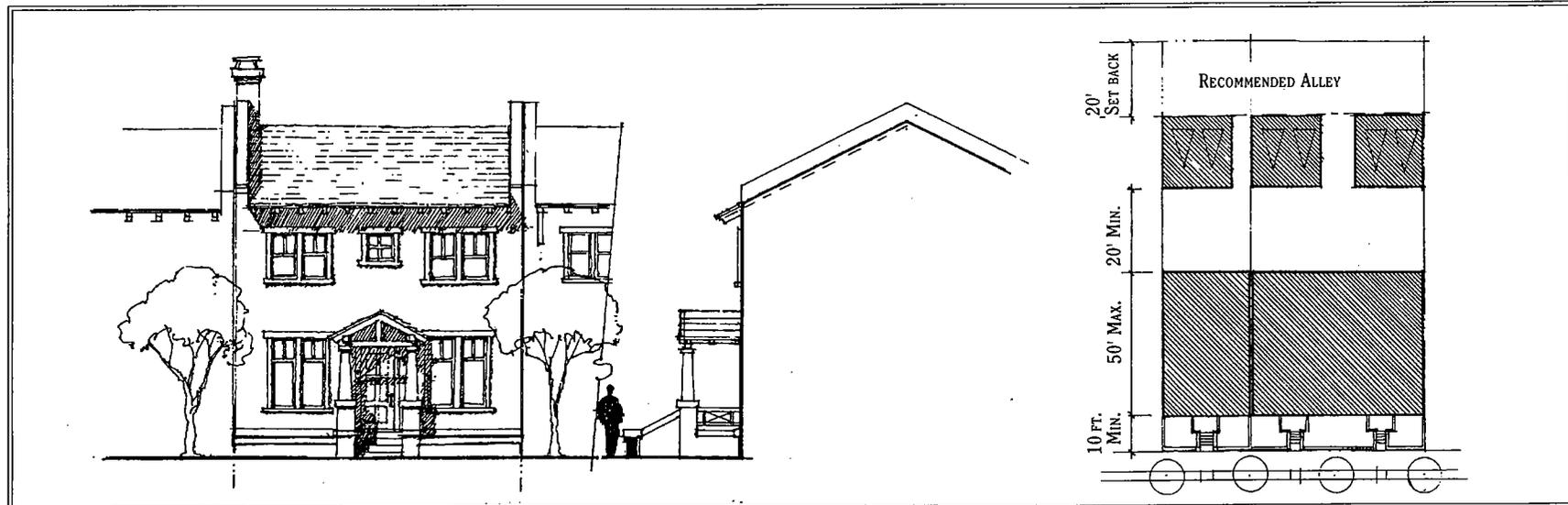
Windows: wood or metal, finished in white or other non-metallic finish, and provided with wood trim and detailing traditional to the building style and design.

- Windows should be inset from the face of the exterior wall a minimum, of three inches to



VILLAGE MIXED-USE TYPE (WITH ALLEY ACCESS)

provide the visual relief of a traditional wood window. Window opening proportions should be square or vertical, NOT horizontal. Windows are often combined in pairs or sets.



TOWNHOUSE TYPE (WITH ALLEY ACCESS)

8. Garage structures should be designed utilizing materials and details substantially similar to the main structure. No more than two, two-car garages may be combined in each building, for a maximum building length of forty-five feet.
9. All fencing will be designed and constructed using materials and details complementary to the building style, color and finish.

Note: Refer to CBD Core Area Sign Guidelines, and the City of Fillmore's CBD Zoning Standards

for specific requirements and allowable sign area.

CBD TRANSITIONAL AREA SIGN DESIGN GUIDELINES

Commercial signs are one of the most important elements of a healthy downtown core. Signage communicates more than just the name of a business. It communicates the degree to which the businesses in that core are alive and whether or not those businesses still care about the business they are doing. A well maintained sign, with bright colors and lighting that is functional, says to poten-

tial customers "We are here, we care and we want your business". Likewise, a dull and worn sign says "We are tired and may not be around much longer - your business isn't that important to us." Unfortunately, many downtowns suffer from too many of the latter and not enough of the former.

These guidelines are intended to encourage excellence in signage, as a communication tool and as an art form, to attract customers, make a statement about the quality of the businesses represented, and to contribute positively to the over-all quality of the downtown core. Businesses are encouraged to use signage in a manner that will clearly express the spirit of their unique enterprises while at the same time enriching the visual life of the downtown street for everyone.

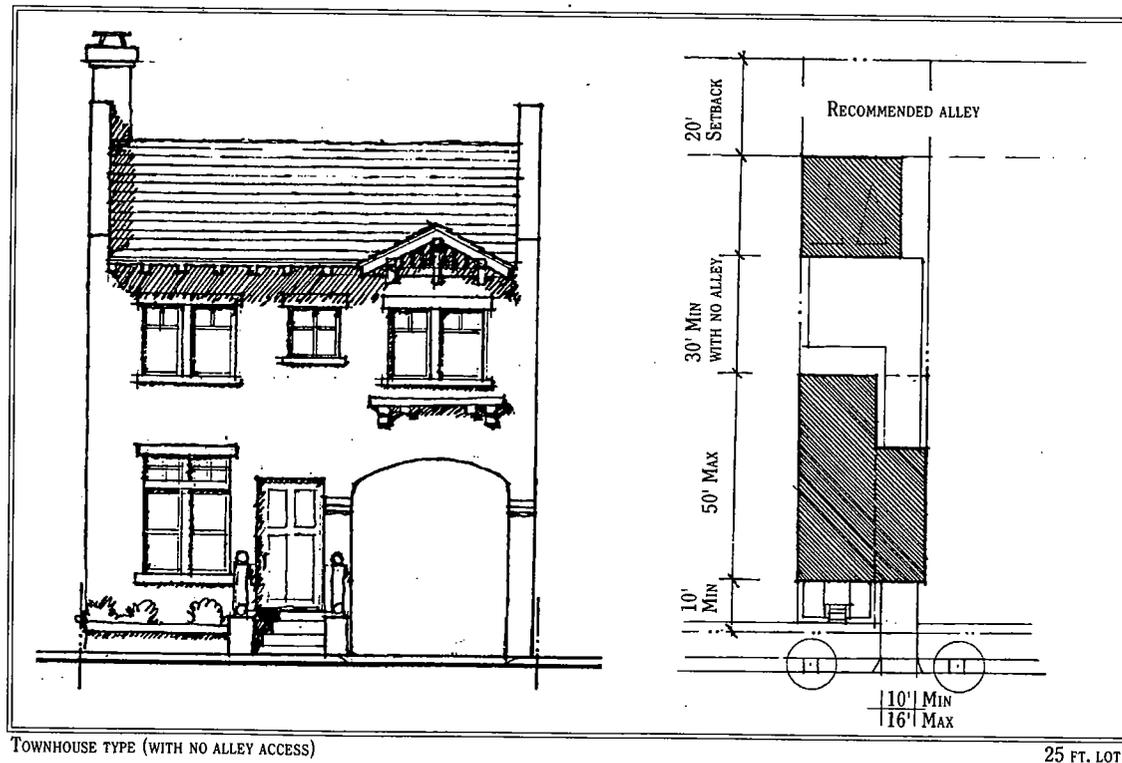
Note: Refer to City of Fillmore's CBD Zoning Standards for specific requirements and allowable sign area.

TYPES OF PERMITTED SIGNS

The following sign types are permitted within the CBD:

WALL SIGNS - wall signs should be located on the building face in an area specifically intended for signage, or where such signage will appear integral to the building design. Such signs may be as follows:

1. Flush-mounted - attached panel or individually mounted letters.





- i. such signs should not block or cover any significant architectural features or details of the building.
 - 2. Painted - applied directly onto the building face in an appropriate location.
 - 3. "Can" or cabinet type wall signs are not permitted.
- B. PROJECTING SIGNS** - may be attached to the building such that no portion of the sign extends lower than seven (7) feet above adjacent finished grade, nor may any portion extend beyond three (3) feet from face of building wall.
- 2. The sign face and structural support should be designed as a compatible unit and should be attached to the building in a manner which will not damage or interfere with the building character.
- C. FIGURATIVE SIGNS** - signs which advertise the occupant business through the use of graphic or crafted symbols, such as shoes, keys, glasses, books, etc. are encouraged. Figurative signs may be incorporated into any of the allowable sign types identified here.
- D. AWNING AND CANOPY SIGNS** - such signs should be integrated into the design of the awning or canopy, subject to the following:
- 1. Sign area should be limited to fifty (50) percent of the total awning or canopy area.

2. Internal or "back-lit" illumination is not recommended.

- E. APPLIED WINDOW SIGNS** - window signs must be applied or painted directly on to the glass, and should not exceed fifty (50) percent of the window area at street level, or twenty-five (25) percent of the window at the second floor.

- 1. Window signs may not be painted over an opaque background, in order to preserve transparency of the window.

- F. DIRECTIONAL, ALLEY AND SERVICE SIGNS** - such sign should be discreet and integrated into the design of the building being served.

DESIGN INTEGRATION

In all cases, the location, style and detail of the sign should be compatible with, and reflect the spirit and quality of, the City of Fillmore's objectives for the Downtown Area, utilizing text, graphics and design features which promote the image of "small town America's Main Street."

- A. SIGN LIGHTING** - Lighting should be considered as an integral part of sign design, and may be either internal or external, with the following considerations:
- 1. Where light fixtures are visible, they should be compatible in character with both the sign and the building design.

- 2. Outside lighting must be directional, and should be positioned such that no glare or hazard is created for onlookers.

B. RECOMMENDED MATERIALS

The following materials are recommended for use in signs displayed in the CBD:

- 1. Wood - carved, sandblasted, etched, and properly sealed, primed and painted, or stained.

- 2. Metal - formed, etched, cast, engraved, and properly primed and painted or factory coated to protect against corrosion.

- 3. High density pre-formed foam or similar material - new materials may be very appropriate if properly designed in a manner consistent with these guidelines, and painted or otherwise finished to compliment the architecture.

- 4. Custom neon tubing, in the form of graphics or lettering, may be incorporated into several of the above permitted sign types.

- 5. Unique materials and designs which are not identified here, but which are compatible with the objectives of this Specific Plan may be considered, subject to review and approval by the Planning Director.



RAILROAD THEME AREA, LOOKING WEST TOWARD CENTRAL PARK

C

RAILROAD PROPERTY

CHARACTER AND SCALE OF THE RAILROAD PROPERTY

The Railroad Property was the site of the original Southern Pacific Railroad Depot, established to provide rail service to the local citrus industry. The property was only partially developed during its use by the rail road, and aside from the City's Central Park and some scattered industrial and auto service uses, has remained mostly undeveloped for nearly a century as the town has grown up around it. This Plan identifies several uses distinct from those of the CBD, and therefore establishes appropriate and differing design requirements for three separate portions of the Railroad property:



RAILROAD PROPERTY BUILDING DESIGN GUIDELINES

COMMERCIAL, BETWEEN CENTRAL AVENUE AND FILLMORE STREET

This portion of the Railroad Property is to be developed consistent with the Design Guidelines for the Central Business District. That is, in addition to the expanded Central Park, new buildings fronting on the East side of the park, west of Fillmore Street, should be developed with a *strong*

urban character compatible with important buildings of the CBD, such as the bank building at the corner of Central Avenue and Main Street and the Masonic Temple at the corner of Central and Sespe Avenues.

It is intended that the architecture of these two buildings should be compatible with the CBD while also serving as a demarcation and transition between the CBD and the Railroad theme development. Together the two buildings will frame a "gateway" at

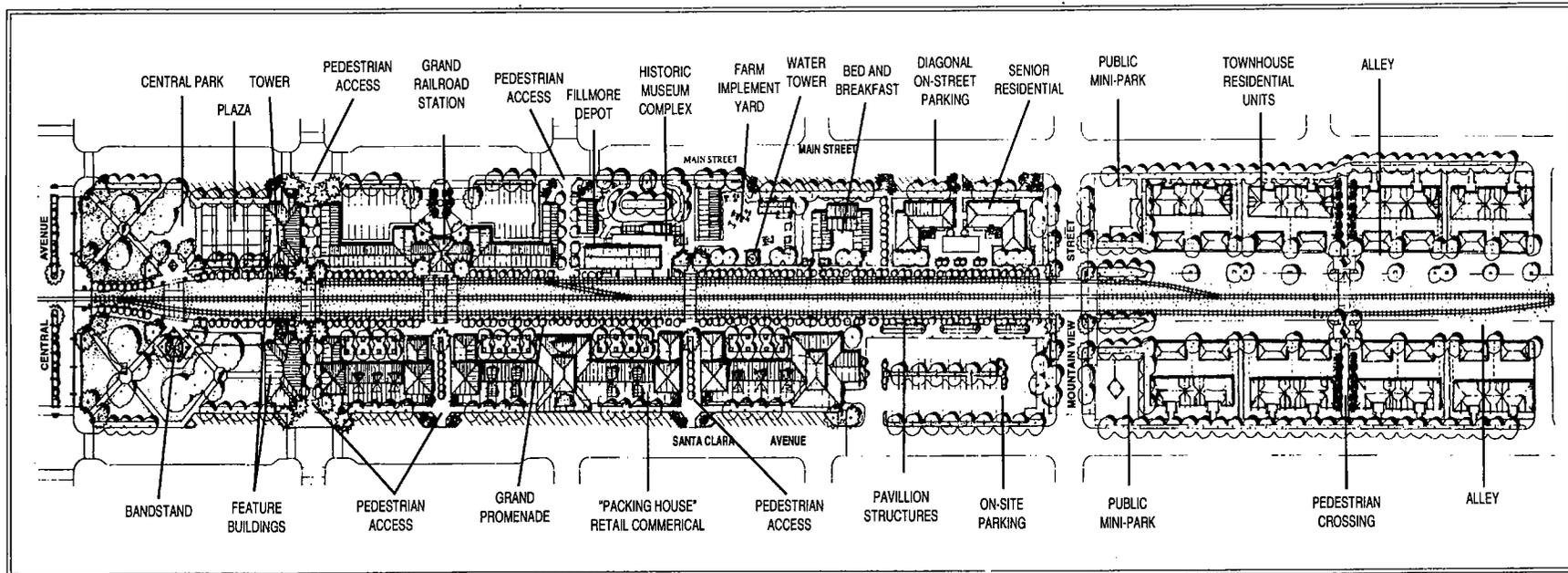
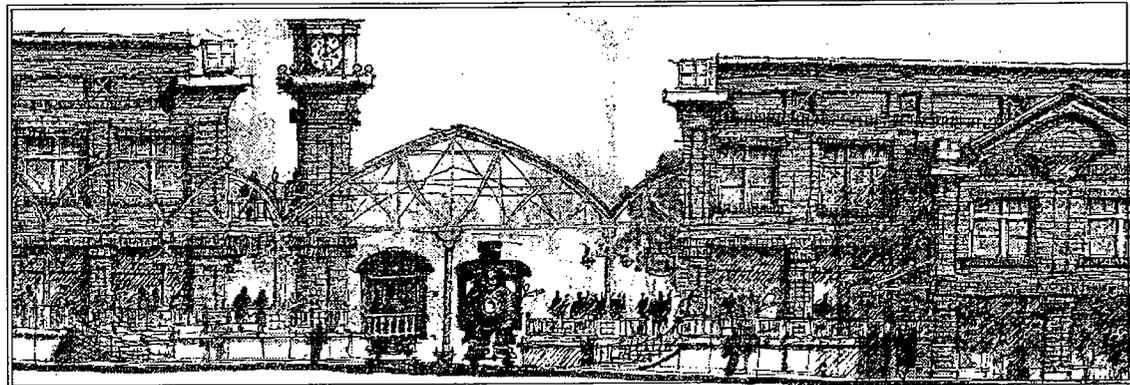


FIGURE 4.54



the Promenade into the Railroad Property development. As an important “urban marker” identifying this gateway, the building located on the northerly side of the tracks should incorporate a substantial tower element at the entry to the Promenade.

BUILDING DESIGN CRITERIA

To assure that buildings of an appropriate character and quality are achieved at this important location, the following Design Criteria should be applied to both buildings, in addition to the CBD Design Guidelines:

1. These buildings will be required to respond equally to the park frontage and to the Railroad Theme development to the east - that is, the property will be assumed to have two front property lines for build-to purposes;
2. The primary exterior building material should be of brick or other high quality masonry finish;
3. The buildings should be of identical or very similar design, sharing like proportions, details, materials and colors;
4. The floor to floor height should be a minimum of sixteen (16) feet.
5. Ample storefront and entries should be provided to encourage a high degree of pedestrian interaction with the buildings at street level, in accordance with the CBD Design Guidelines;

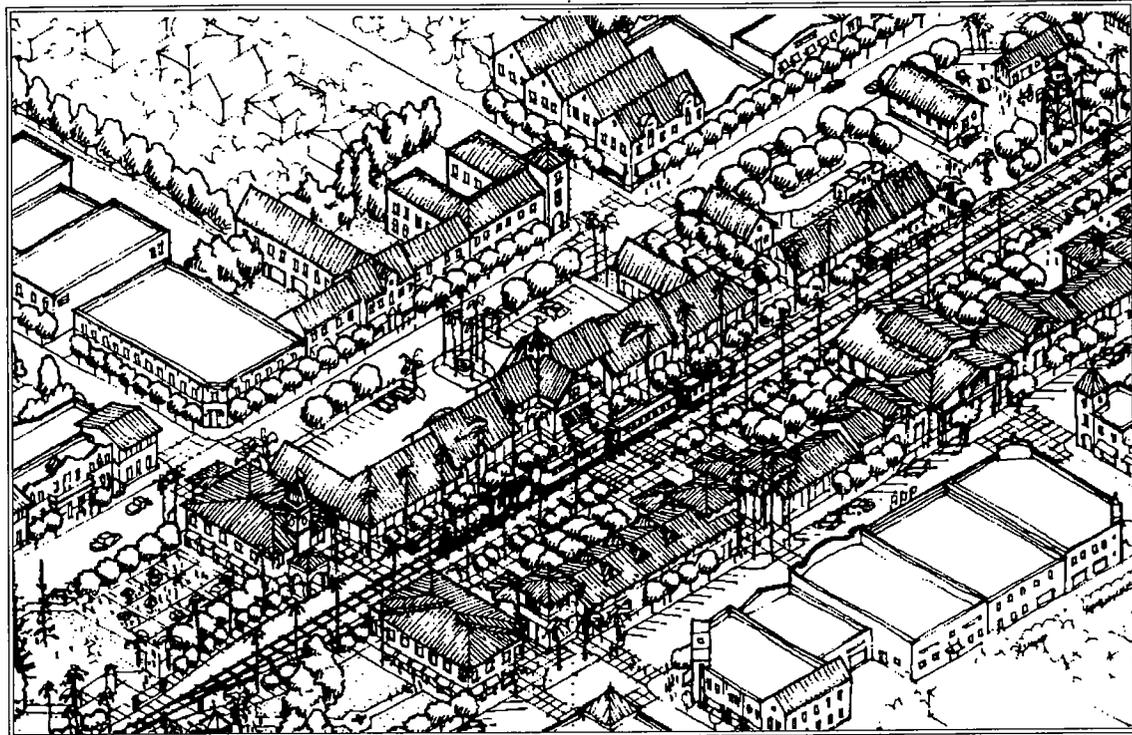


6. It is strongly recommended that the buildings be slightly raised above the park elevation, and that they utilize a "public" terrace as a transition between the activity of the park and that of the commercial uses at the buildings ground floors.

Refer to the CBD Design Guidelines for additional design criteria not addressed here.

COMMERCIAL AND VISITOR SERVING, BETWEEN FILLMORE STREET AND MOUNTAIN VIEW

It is a goal of this Specific Plan that this portion of the Railroad Property should be developed in a manner supportive of tourist and locally serving commercial uses centered on a "Railroad-Oriented Design Theme". Buildings within this portion of the property should provide a backdrop of railroad related architecture, reflecting an industrial or civic railroad character appropriate to the 1920's and 1930's small town character identified for the CBD. One building representing the "Grand Railroad Station" should present a prominent and highly civic appearance, very compatible with the CBD. Other buildings should emulate a more utilitarian or packing house character, not without detail and prominence, but of a simpler nature, as illustrated in the Railroad Property Design Guidelines below.

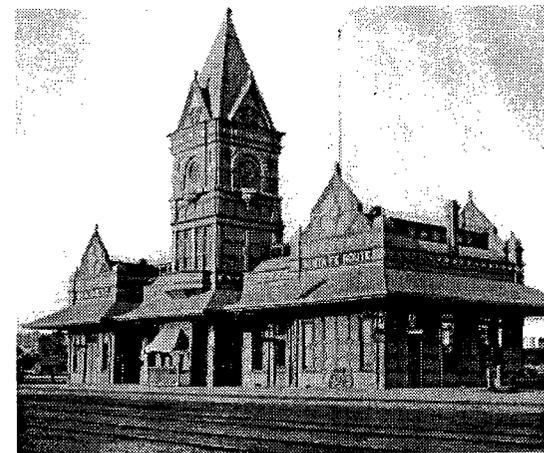


THE RAILROAD PROPERTY WILL BE DEVELOPED IN A MANNER CENTERED ON A RAILROAD ORIENTED DESIGN THEME

FIGURE 4.54

"GRAND RAILROAD STATION"

It is intended that this building become the focal point of the Railroad Theme development, and as such it should achieve the design importance of a civic building. The Concept Plan shows this building at the western edge of the Railroad Theme development, on the north side of the tracks. Here, the "Station" will serve to complete the transition from the CBD to railroad-oriented design. This will best be accomplished by requiring the specific design criteria below.



Building Design Criteria

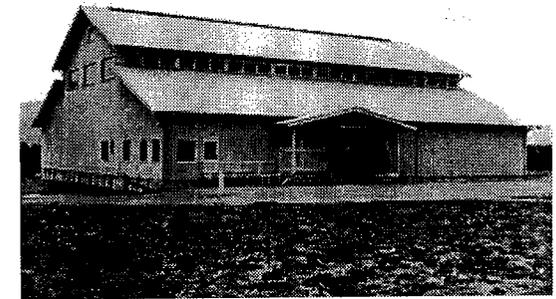
1. The architecture of this building should closely resemble the character of the CBD buildings to the immediate west in material and detail, with the exception that pitched roof forms should be a dominant element of the design;
2. The main body of the building should be set back from the public street, as indicated on the concept plan, and aligned with other railroad-oriented buildings to strongly address the promenade.

3. The design should incorporate some form of covered outdoor "waiting area", oriented to the promenade;

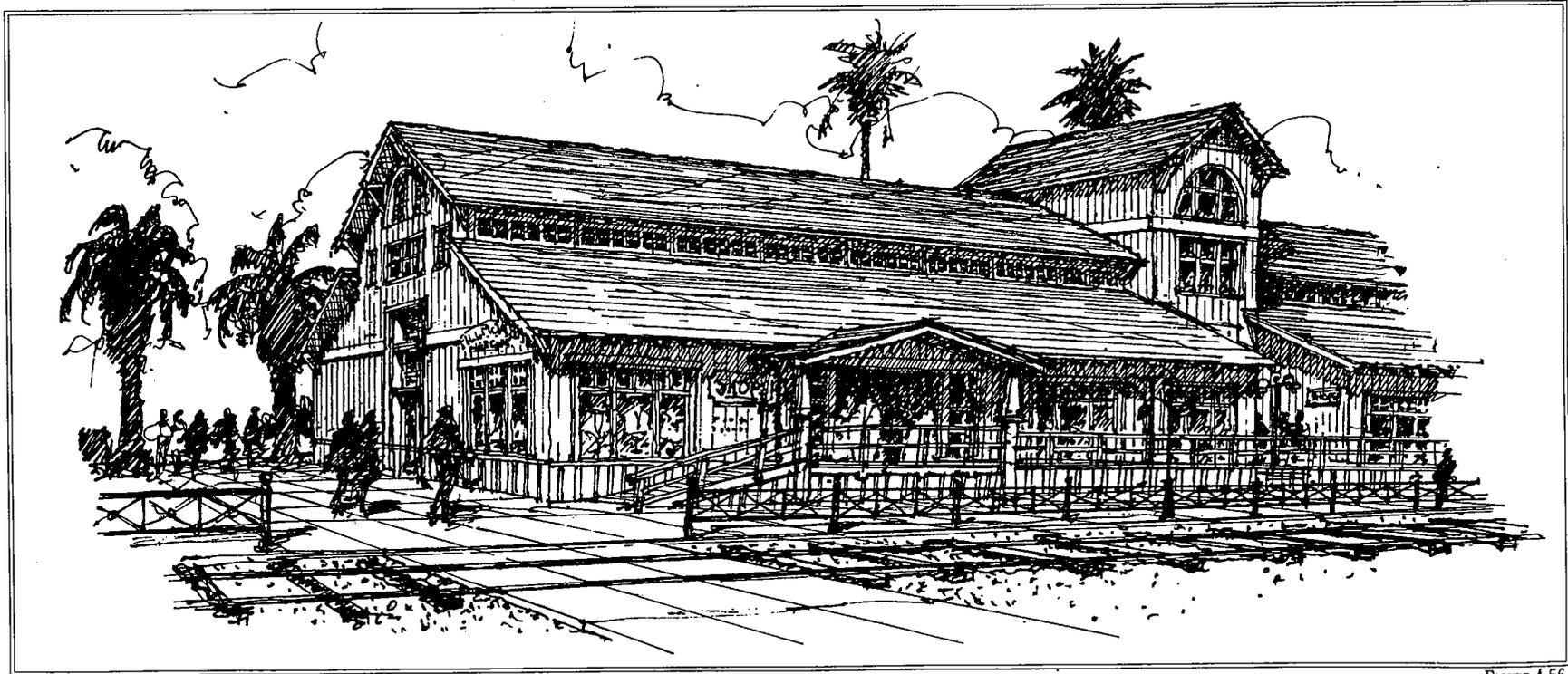
Refer to the CBD Architecture Design Guidelines for additional design criteria not addressed here.

"PACKING HOUSES AND SUPPORT BUILDINGS"

Unlike the buildings of the CBD, these buildings are to reflect the simple forms typified by the



THE OLD PACKING HOUSE AT RANCHO SESPE, NEAR FILLMORE.



"PACKING HOUSE" COMMERCIAL BUILDINGS ARE TO REFLECT THE SIMPLE FORMS AND CHARACTER TYPIFIED BY THE CITRUS INDUSTRY PACKING HOUSES.

FIGURE 4.56



citrus industry packing houses and other railroad support buildings, incorporating larger one and two story masses with pitched roofs, overhangs, and heavier more rustic detailing than will be found in the CBD. The design challenge is to successfully integrate the lively pedestrian commercial uses desired at the promenade level into this building type. There are several similar examples of successful projects which can be found throughout the United States. Buildings of the "Cannery Row" in Monterey, both new and restored originals, offer perhaps the most famous example of this unique blending of building type and use. The specific design criteria below should apply to all buildings, with the exception of the "Grand Railroad Station", within the Railroad themed area:

Building Design Criteria

The architecture of this portion of the property may best be typified by the style of the Fillmore Depot. Although it is intended that most other buildings will be larger, they will share the simple, rustic and functional aesthetic of broad eaves, exposed rafters, and clap board siding. The masses of building elements should be simple and voluminous, broken by occasional changes in wall plane and the provision of dock elements (actually usable decks, ramps, steps and landings).

Building design at the street level should provide the strongest pedestrian orientation inward, toward the promenade, by providing many entries,

windows, signage and other amenities to encourage pedestrian activity and exploration along the length of the promenade walk. However, building design must also assure that a high degree of architectural interest is provided at the perimeter of the project as well. This may be accomplished by including architectural design features such as entries, decks, eaves and overhangs, and by "announcing" pedestrian access points from the public street into the project with unique design treatment, wherever they occur. In addition, the following criteria should apply at street level:

1. On the Promenade frontage at least one building entrance should be provided every fifty (50) feet, although more points of access are highly



A FILLMORE PACKING HOUSE, 1930'S

recommended, as they encourage a higher level of pedestrian activity.

2. On the public street frontage, buildings are not required to meet the "build-to" line of the CBD. Some variation, as suggested by the Concept Plan, is encouraged. Building entries or windows are encouraged to be provided as often as is practicable to provide public access and visibility for occupant businesses within.
3. Wall area between entrances should include ample amounts of storefront or display windows.
4. Storefront openings (the space or opening between columns or other solid building mass, in which storefront windows and/or doors are located) should be a maximum of twenty (20) feet in width. Storefront systems within these openings should be designed in substantial conformance with the CBD storefront design guidelines.
5. Features, such as gables, finials, dormer vents, etc. are encouraged where deemed appropriate to the character of the building design.
6. Highly prominent architectural features, such as towers and turrets should be reserved for buildings occupying key locations within the project, such as the point of connection for a pedestrian entrywalk or on axis with a perpendicular street.



RUSTIC PACKING HOUSE INTERIOR, 1930's.

7. Each building should be provided with an architectural "base" in accordance with the CBD Guidelines. In some conditions within the Railroad property, the building may actually be raised above the ground level to accommodate a loading dock, deck area, basement, etc. In such instances, the "base" becomes a skirting element to surround and screen the structure under the elevated building. Within the Railroad theme area, wood may be used as a base, provided it is appropriately treated and integrated into the over-all building design;
8. Service Entries are those entries which are not intended for use by the general public. The service entry should be designed to blend into the surrounding building facade, provided with simple detailing, trim and finish consistent with the character of the building, such that it should not draw particular attention to itself, but should remain attractive and easy to identify;
9. Where trash and or storage areas are provided in conjunction with service entries, such areas should be completely enclosed and screened from public view, by the use of walls, doors and gates designed in a manner consistent with the architecture of the buildings;



10. Exterior Building Materials should be selected based upon appropriateness to downtown Fillmore's early twentieth century character, the architectural style of the particular building, lasting beauty and durability of the finish. In addition to materials allowed in the CBD, wood siding is a permitted exterior wall finish material;
11. Window ratio - the amount of window, other than storefront systems, at street level should occupy between 50% and 60% of the wall area. Above the street level this ratio should fall to between 30% and 50%, depending upon building style and other architectural criteria;
12. Materials for window sash and frame may be of wood or metal, painted or otherwise finished to provide a durable and long lasting surface. Where divided windows are desired, true-divided lites are strongly recommended. "Snap-in" type mullions or grilles which simulate this appearance are not permitted;
13. Clear glazing is preferable, especially where street level display is the objective. Where tinted glazing is used, the tint should be kept as light as possible. Dark windows create the impression of emptiness;
14. Reflective or mirrored glazing is not permitted;
15. Pitched Roofs are required in this portion of the Railroad Property. Roof planes should ter-

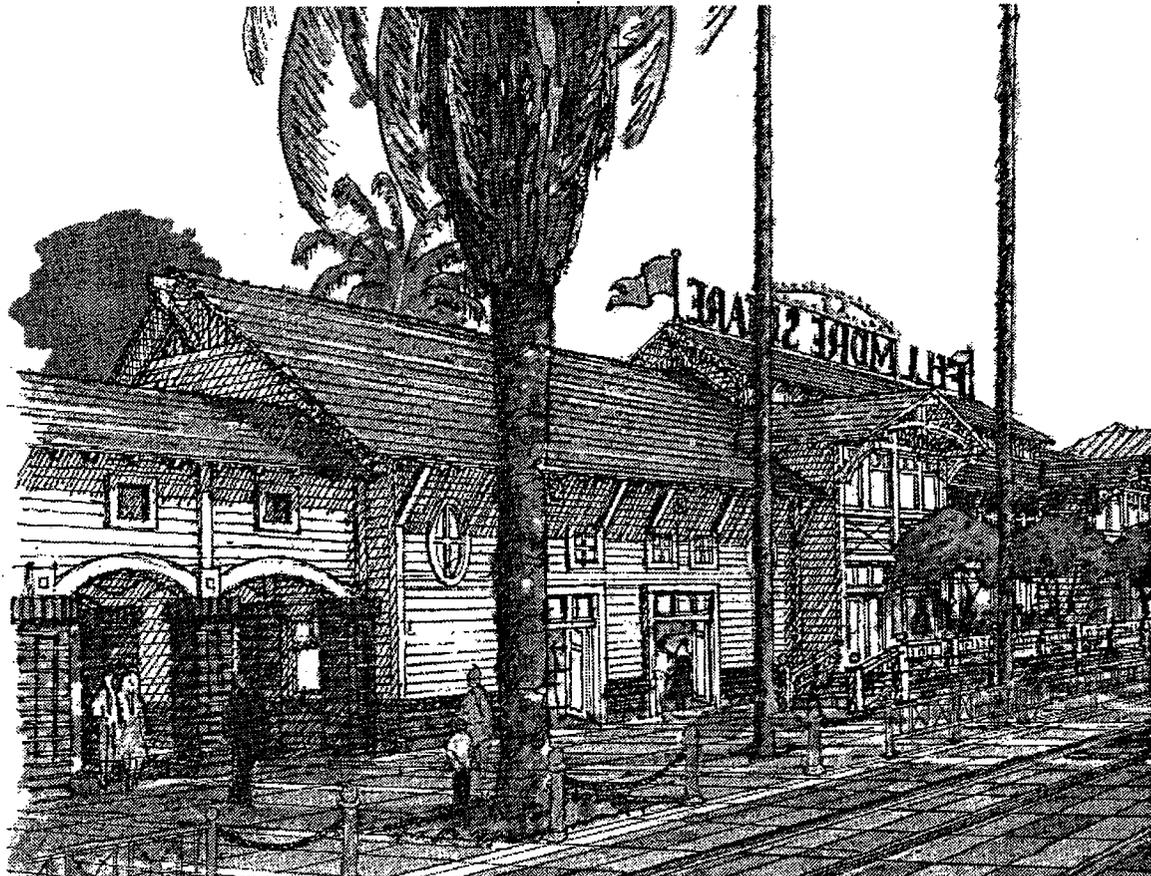
minate with detailed eaves, utilizing eave molding, shaped rafter tails or other means of termination appropriate to the architecture of the building;

16. Dormer type roof elements may be incorporated into sloped roof designs, especially where such roof area occupies a substantial percentage of the buildings linear frontage;

17. Mansard type roofs should be avoided;

18. Acceptable Roofing Materials include:

- a. Concrete tile: commercial grade, of a color and style complimentary to the building design.
- b. Metal seam: standing seam or batten seam, natural copper, anodized or factory



RAILROAD THEME COMMERCIAL BUILDING

coated finishes. Painted metal roofing is not recommended.

- c. Higher grade composition shingles with “dimensional thickness”.

19. “Flat roof” areas should be detailed and of a material to provide a clean, uniform appearance from windows of adjacent buildings.

20. Building Accessories and Details - There are a number of design elements which may be incorporated into the building design, especially at street level, in order to add to the experience of the pedestrian while meeting important functional needs as well. Refer to the CBD Design Guidelines for a list of accessories and details recommended for inclusion in these buildings.



A RAILROAD HOTEL, LATE 1800'S





21. **Exterior Color Design** - Exterior colors play an important role in the way we perceive a building and its details. Colors can be used to draw attention to specific parts of a building, such as entries. Color can also be used to mask or diminish the visual importance of a particular feature, for instance a service area, simply by altering colors, or the contrast between surfaces and details, in a certain area. In general, contrast (light against dark or dark against light) will call for your attention, for better or for worse. Refer to the CBD Design Guidelines for specific recommendations on color treatment.

RESIDENTIAL, EAST OF MOUNTAIN VIEW

The Specific Plan has identified a specific residential type, townhouses, for this portion of the Railroad Property. In addition, the Plan has identified Craftsman style architecture as appropriate for this residential development. These guidelines identify some key design elements which should be considered in the project design to assure compatibility with the City's over-all Design Objectives.

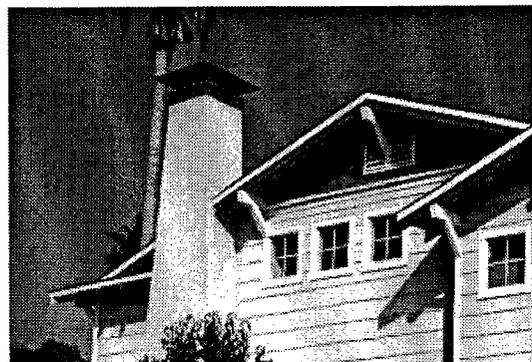
BUILDING DESIGN CRITERIA

1. **Building Mass and Organization** - all multi-unit buildings should be designed to be compatible with the adjacent and nearby neighborhoods to the north, utilizing, or incorporating strong reference to, traditional craftsman scale, detail and materials, as identified below.



EACH DWELLING SHOULD BE CLEARLY EXPRESSED.

FIGURE 4.5



CRAFTSMAN DETAILS ARE SIMPLE AND BOLD.

2. Each individual dwelling should be clearly expressed as a single identifiable unit through the use of pronounced roof forms, changes in building mass, pronounced entryways and feature windows, such as bay or large divided picture windows.

3. Each dwelling should have a front porch, covered and partially enclosed by a decorative rail.

4. Building setback should be between twelve



GARAGES WILL BUFFER PRIVATE YARDS FROM PUBLIC ALLEY.

and twenty-five feet from back of sidewalk. The resulting front yard should be defined by means of decorative low wooden fence or hedge, and provided with a gate and/or entry arbor.

5. Traditional elements, such as columns, exposed eaves, trellises, decorative chimneys and dormer windows should be incorporated into the design.

6. Acceptable materials include:

Exterior walls: wood siding, stucco, brick masonry, stone

Roofs: fiberglass composition roof shingles (higher quality, "dimensional" material only).

Windows: wood or metal, finished in white or other non-metallic finish, and provided with wood trim and detailing traditional to craftsman design.

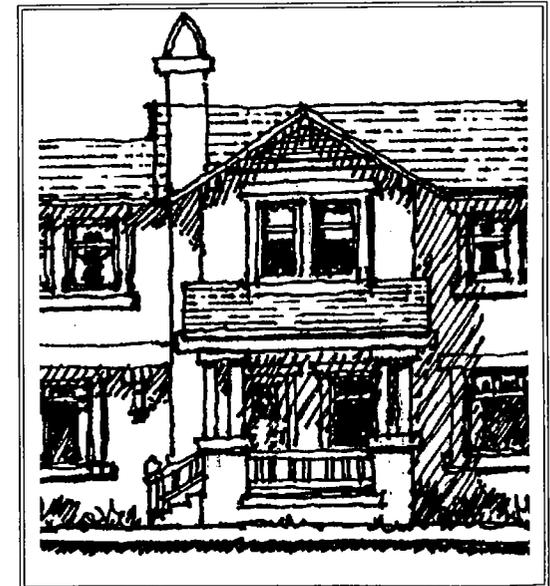


CRAFTSMAN STYLE TOWNHOUSES CONSTRUCTED 1991, SANTA ANA, CA.

7. Windows should be inset from the face of the exterior wall a minimum, of three inches to provided the visual relief of a traditional wood window. Window opening proportions should be square or vertical, NOT horizontal.

8. Garage structures should be designed utilizing materials and details substantially similar to the main structure. No more than two, two-car garages may be combined in each building, for a maximum building length of forty-five feet.

9. Private rear yard area will be defined and enclosed by means of walls or fences which are designed compatibly with the craftsman style in either wood or masonry construction. A rear gate should be provided to provide alley access directly from each rear yard. Exterior lighting should be provided at each gate.



PORCHES DEFINE ENTRY AND OWNERSHIP



RAILROAD PROPERTY ARCHITECTURAL LIGHTING GUIDELINES

Lighting should be considered as an integral part of building design. Proper lighting design will have a positive effect on the building and its users, as well as help to promote a safe and enjoyable nighttime pedestrian environment in the downtown core.

TYPES OF LIGHTING

Within the Railroad Property project area there will be a need for several exterior lighting types:

1. Area Lighting - for public and private parking lots, alley ways, parkways and walks. Such lighting should be set in a manner that assures maximum lighting benefit without allowing stray light to intrude into windows of nearby residents or to create glare problems for nearby automobile traffic.
2. Ornamental Lighting - not intended to light large areas or wall surfaces. Ornamental lighting plays an important role in bringing visual life to streets at night. Colors, finishes and design of ornamental lighting should be compatible with the design characteristics of the architecture.

3. 'Hidden Source' Lighting - for certain prominent architectural features, hidden source lighting can be used to create dramatic effects, illuminating towers or other unique architectural characteristics. Such lighting can be concealed in soffits, behind ledges or parapets, or even set into landscape areas with the light directed at the desired element to be highlighted.

DESIGN INTEGRATION

It is recommended that a lighting design professional be consulted as an integral part of the design process for determining types and intensities of lighting to be used. In addition, the following 'rules of thumb' should apply:

1. Use the minimum brightness necessary for illumination of large areas.
2. Use brighter, more intense lighting to highlight architectural features and focal points.

MATERIALS

Lighting fixture materials should be compatible with the design objectives of this Specific Plan.

1. Where lighting is mounted to a building, its materials, finishes and colors should complement and coordinate with those of the building.

2. Recommended Lighting Glass and Globes include:

- a. clear acrylic or poly carbonate with fresnal patterns;
- b. borosilicate glass;
- c. translucent clear-frosted or white acrylic or polycarbonate globes.

3. Glass and Globes which are *not* recommended:

- a. clear or tinted smooth finish acrylic or polycarbonate globes;
- b. any dark-tinted or mirrored glass diffusers

4. Recommended lamp types include:

- a. color corrected high pressure sodium (HPS);
- b. color corrected metal halide (3,000 degrees K);
- c. color corrected fluorescent (2,700 -3,000 degrees K).

5. Lamps which are *not* recommended include:

- a. Standard "peach" high pressure sodium;
- b. low pressure sodium;
- c. standard mercury vapor;
- d. non-color-corrected, standard fluorescent.

It is recommended that a professional lighting consultant be involved in the project where business success can be greatly assisted by quality lighting and display.



RAILROAD PROPERTY SIGN GUIDELINES

These guidelines are intended to encourage excellence in signage, as a communication tool and as an art form, to attract customers, make a statement about the quality of the businesses represented, and to contribute positively to the over-all quality of the downtown core. Businesses are encouraged to use signage in a manner that will clearly express the spirit of their unique enterprises while at the same time enriching the visual life of the downtown street for everyone.

Note: Refer to City of Fillmore's CBD Zoning Standards for specific requirements and allowable sign area.

TYPES OF PERMITTED SIGNS

The following sign types are permitted within the Railroad Property Theme Development area:

A. WALL SIGNS

Wall signs should be located on the building face in an area specifically intended for signage, or where such signage will appear integral to the building design. Such signs may be as follows:

1. Flush-mounted - attached panel or individually mounted letters.
 - a. such signs should not block or cover any significant architectural features or details of the building.
2. Painted - applied directly onto the building face in an appropriate location.
3. "Can" or cabinet type wall signs are not permitted.

B. PROJECTING SIGNS, MARQUEES

Projecting signs and marquees may be attached to the building such that no portion of the sign extends lower than seven (7) feet above adjacent finished grade, nor may any portion extend beyond three (3) feet from face of building wall.

1. Projecting signs may be directly attached to the building face, or suspended from decorative brackets, as illustrated in Figure 4.52.
2. The sign face and structural support should be designed as a compatible unit and should be attached to the building in a manner which will not damage or interfere with the building character.

C. FIGURATIVE SIGNS

Signs which advertise the occupant business through the use of graphic or crafted symbols, such

as shoes, keys, glasses, books, etc. are encouraged. Figurative signs may be incorporated into any of the allowable sign types identified here.

D. AWNING AND CANOPY SIGNS

Such signs should be integrated into the design of the awning or canopy, subject to the following:

1. Sign area should be limited to fifty (50) percent of the total awning or canopy area.
2. Internal or "back-lit" illumination is not recommended.

E. APPLIED WINDOW SIGNS

Window signs must be applied or painted directly on to the glass, and should not exceed fifty (50) percent of the window area at street level, or twenty-five (25) percent of the window at the second floor.

1. Window signs may not be painted over an opaque background, in order to preserve transparency of the window.

F. DIRECTIONAL, ALLEY AND SERVICE SIGNS

Such sign should be discreet and integrated into the design of the building being served.



G. MURALS

Murals will be allowed only on blank side walls of buildings, and are subject to review and approval of the Planning Director.

H. ROOF SIGNS

Roof signs will be permitted on limited buildings within the Railroad Property, only where such sign can be shown to specifically add to the total project quality and image. At this time one roof sign of substantial size is anticipated to be incorporated into the design of the primary "packing house" commercial building, subject to the following criteria:

1. The sign shall identify the official name of the Railroad Property project, as approved by the City of Fillmore;
2. The height of the sign characters is anticipated to be approximately 6 feet, but such height may be increased or decreased as determined appropriate by the Community Development Department.

DESIGN INTEGRATION

In all cases, the location, style and detail of the sign should be compatible with, and reflect the spirit and quality of, the City of Fillmore's objectives for the Downtown Core, utilizing text, graphics and design features which promote the image of "small town America's Main Street."

A. SIGN LIGHTING

Lighting should be considered as an integral part of sign design, and may be either internal or external, with the following considerations:

1. Where light fixtures are visible, they should be compatible in character with both the sign and the building design.
2. Outside lighting must be directional, and should be positioned such that no glare or hazard is created for onlookers.

B. RECOMMENDED MATERIALS

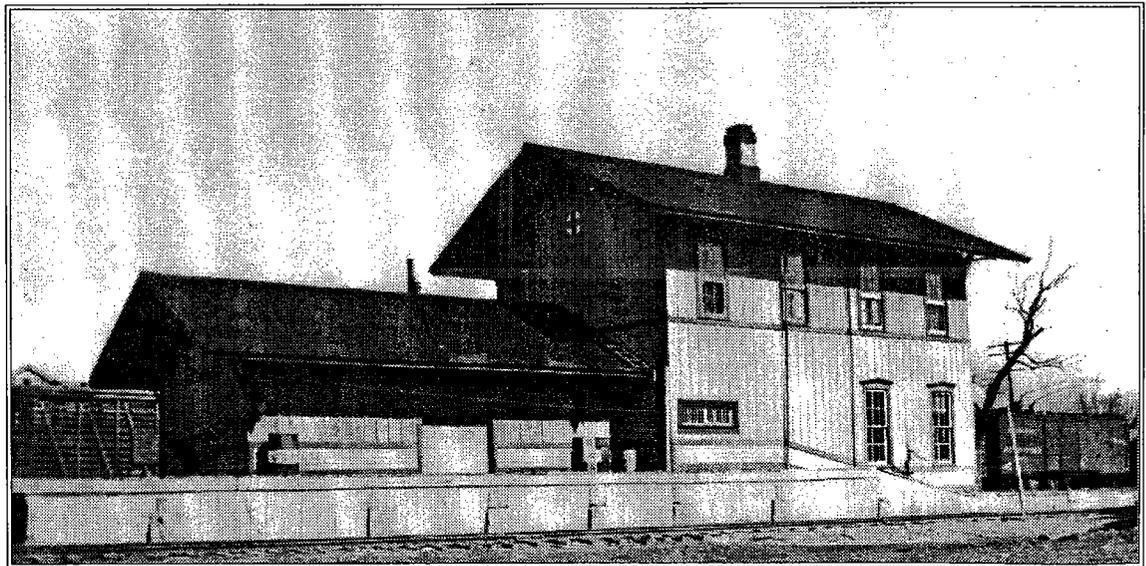
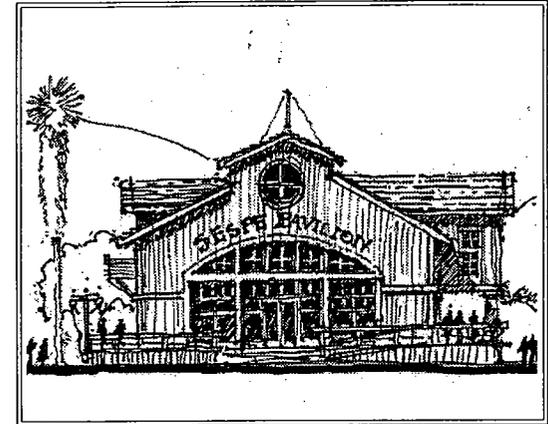
The following materials are recommended for use in signs displayed in the CBD:

1. Wood - carved, sandblasted, etched, and properly sealed, primed and painted, or stained.



ONE ROOF SIGN OF SUBSTANTIAL SIZE IS TO BE INCORPORATED INTO THE DESIGN OF THE "PACKING HOUSE" BUILDINGS FOR PROJECT IDENTIFICATION.

2. Metal - formed, etched, cast, engraved, and properly primed and painted or factory coated to protect against corrosion.
3. High density pre-formed foam or similar material - new materials may be very appropriate if properly designed in a manner consistent with these guidelines, and painted or otherwise finished to compliment the architecture.
4. Custom neon tubing, in the form of graphics or lettering, may be incorporated into several of the above permitted sign types.
5. Unique materials and designs which are not identified here, but which are compatible with the objectives of this Specific Plan may be considered, subject to review and approval by the Planning Director.



A STATION WITH FREIGHT BUILDING AND DOCK



RAILROAD PROPERTY LANDSCAPE DESIGN GUIDELINES

The rail road has been identified as a significant element in Fillmore's history and as such, thematic development within the rail road property provides the opportunity to celebrate Fillmore's Heritage. The intent of these Landscape Design Guidelines is to develop this historic rail road theme. Generally, The Rail Road Property Landscape Design Guidelines are a more refined and detailed application of the CBD Streetscape Standards and Guidelines.

1. THEME COMMERCIAL AREA

The Rail Road theme should reach its hallmark within this area. Adjacent residential areas within the rail road property should reflect the theme in a more restrained manner. A primary goal is to create a quality environment which will be an attraction for visitors, provide for the community's social needs and instill within the community a sense of civic pride.

A. GENERAL DESCRIPTION

The Theme Commercial area is bordered by Central Park, Main Street, Santa Clara

Street, and Mountain View Street. Primary pedestrian zone circulation in this area is provided by the pedestrian Promenade. Access to the promenade is via Central Park, parking lots, on-street crossings and pedestrian street crossings. All these areas are considered secondary pedestrian zones to be provided with landscape treatments which guide the pedestrian to the Promenade, the primary pedestrian zone.

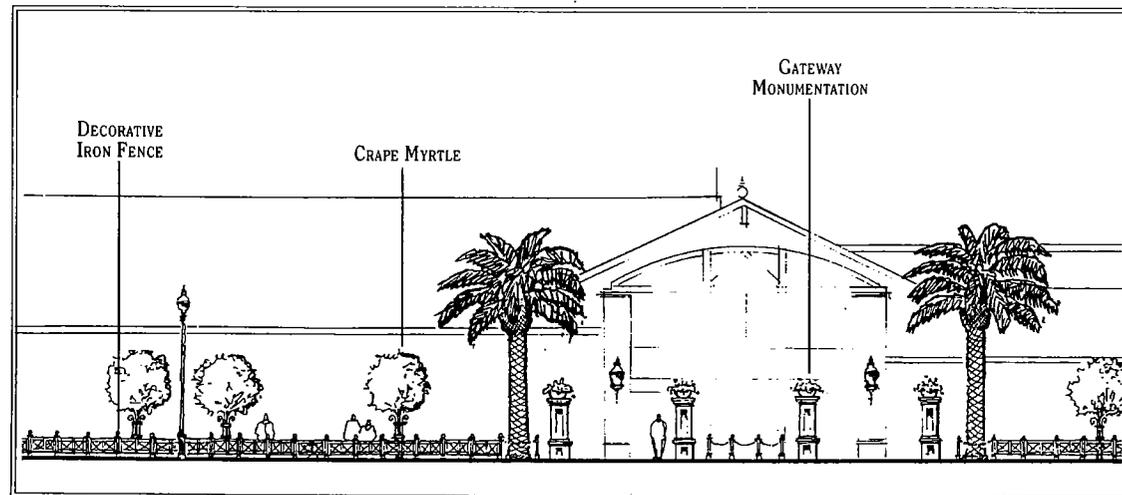
B. PEDESTRIAN ZONES

Special attention needs to be given to the Promenade. This primary pedestrian zone, along with Central Park, is to be given the richest level of landscape development.

1. Primary Pedestrian Zone - The Promenade, in conjunction with Central Park, functions as the

ultimate of pedestrian spaces in the CBD. Design materials utilized are to be the richest of those used throughout the CBD. The design composition should be at the highest level within the Theme Commercial area. Sensitive development of the rail road theme is to be achieved through the use of turn of the century materials such as iron and brick.

2. Secondary Pedestrian Zone - These areas provide pedestrian access to the Promenade and are subordinate in design and detail development. These zones are to have a supportive role to the Promenade. Design materials should be consistent or complementary to those used on the promenade. They should also respond to the specific buildings and activities while supporting the strength of the overall theme.



RAILROAD THEME DEVELOPMENT ELEVATION AT PRIMARY PEDESTRIAN CROSSING

FIGURE 4.54

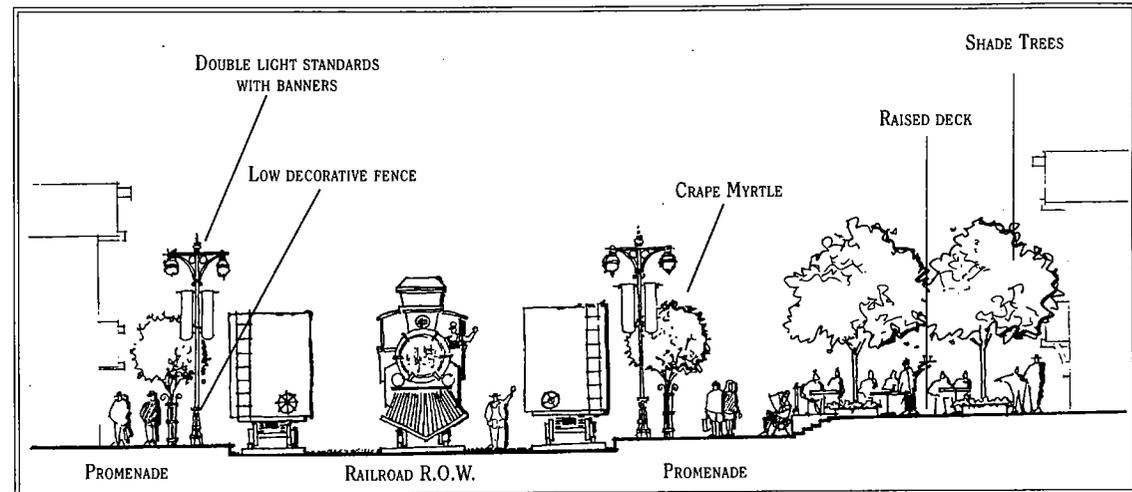
C. GATEWAYS

1. Primary Gateway - The development of primary gateways should occur at pedestrian crossings of the railway. These gateways should reflect those described for the CBD in chapter 5. These nodes along the promenade should be defined with thematic columns, specimen palms, enriched paving and bollards, per these guidelines.
2. Secondary Gateway - Points of intersection between secondary pedestrian zones and the promenade, perimeter streets, and parking areas should be developed as gateways. The level of development of these areas should be more restrained than the primary gateway.

D. DESIGN MATERIALS

The previously described design components should be detailed with materials consistent with the historic fabric of Fillmore, as identified in the Building Design Guidelines of this Specific Plan. These design materials are also described in depth in chapter 5. The following materials are to be used in the Rail Road Property Theme Commercial Area:

1. Fencing along the Railway - Three foot high wrought iron fencing separates pedestrians from the track area and provides the opportunity for thematic detailing. Care should be taken to integrate the fencing with the other design materials of the promenade especially bollards, light standards, and tree arbor guards.



CROSS SECTION THROUGH RAILROAD DEVELOPMENT LOOKING EAST

FIGURE 4.55

2. Promenade Tree Planting - Crape myrtle with arbor guards should be paired around light standards provide a human scale and seasonal color. This planting is alternated with a colonnade of Mexican fan palms which provide a dramatic skyline element defining the area. Canary island date palms should be utilized at primary gateway locations to provide the gateways with a monumental quality.

Southern magnolia trees may be substituted for the Canary island date palms. The pros and cons of using palms in this area must be given serious consideration as they can present significant problems for movie filming, since this activity often aims to create the illusion of other regional environments where palms would not be located.

2. RESIDENTIAL AREA

Separated from the theme commercial area by Mountain Avenue, the residences within the railroad property should be given a sense of continuity of the Theme Commercial Area through proper landscape development. In addition to providing a sense of continuity, the landscape is to be residential in scale.

A. GENERAL DESCRIPTION

The railroad property residential area is bordered by Mountain View Street, Santa Clara Street and Main Street. Vehicular access to garages is adjacent the railway right of way. Corner parks along Mountain View Street provide a physical and visual buffer from the Theme Commercial Area.



B. CORNER PARKS

These parks are to be located on the north and south sides of the railway and provided with a crossing to thematically mirror the pedestrian railway crossings along the promenade. Both parks should be developed with dense buffer plantings adjacent to the vehicular access and adjacent residential units, and are to incorporate the following design elements:

1. North Park

- Open lawn area
- Picnic arbor structure with tables and turn of the century detailing

2. South Park

- Open lawn area
- Pavilion with benches and turn of the century detailing
- Tot lot

C. PEDESTRIAN ZONES

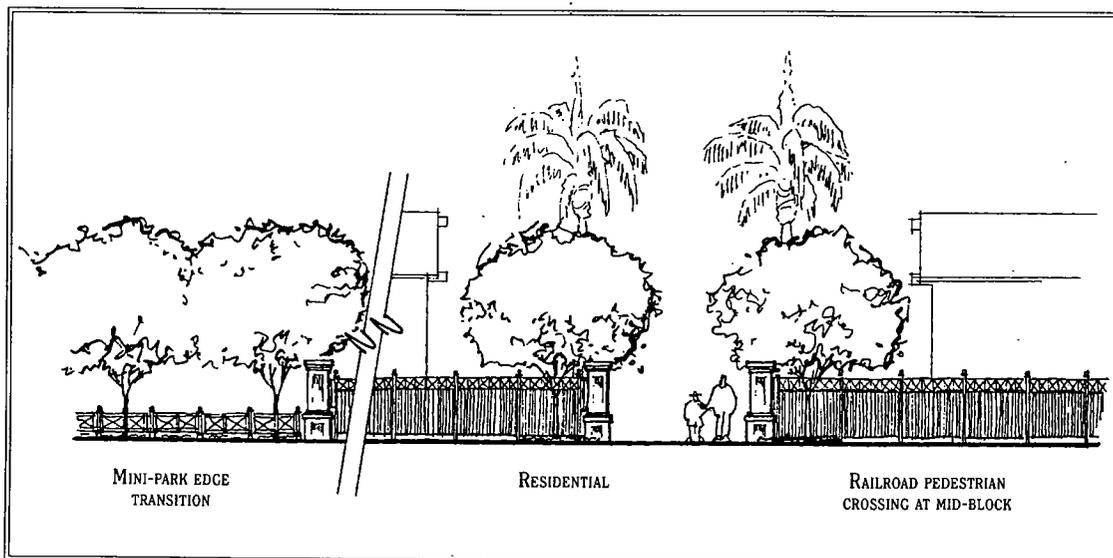
1. Sidewalk Development - Where space allows, as determined by the City Engineer, parkways are to be incorporated. A dense planting of a consistent canopy trees should be provided in the parkway. Where space is limited, the trees should be planted in tree wells. Foundation plantings should complement the street tree plantings, incorporating tree and shrub plantings which help to create a pleasant pedestrian experience.

2. Pedestrian Railway Crossings - At Mountain View Street, Landscaping improvements are to mirror the Theme Commercial Area. The central crossing at the center of the development should be defined with thematic columns more residential in scale. The thematic date palms are substituted with a less monumental canopy type tree.

D. DESIGN MATERIALS

A discussion of design materials to be utilized in developing the above described design components is found in chapter 5. The following materials are to be used in the Residential Area within the Railroad Property:

1. Fencing along the Railway - Starting at Mountain View Street, three foot high wrought iron fencing is to run adjacent to the railway between the thematic columns, separating the parks and the Rail Road right-of-way. At the end of the vehicular gateway of canopy trees, thematic columns are to provide a transition to the six foot high wrought iron fencing separating the Rail Road from the residential alley. This 6 foot fencing continues to the central pedestrian crossing and then beyond to the end of the property. The fencing should be detailed to match that used in the Theme Commercial Area.



RESIDENTIAL CROSSING, LOOKING NORTH

FIGURE 4.56